

NOTICE OF MEETING

PLANNING SUB COMMITTEE

Monday, 8th February, 2021, 7.00 pm - MS Teams (watch it [here](#))

Members: Councillors Sarah Williams (Chair), Gina Adamou (Vice-Chair), Dhiren Basu, John Bevan, Luke Cawley-Harrison, Justin Hinchcliffe, Peter Mitchell, Sheila Peacock, Reg Rice, Viv Ross and Yvonne Say

Quorum: 3

1. FILMING AT MEETINGS

Please note this meeting may be filmed or recorded by the Council for live or subsequent broadcast via the Council's internet site or by anyone attending the meeting using any communication method. Although we ask members of the public recording, filming or reporting on the meeting not to include the public seating areas, members of the public attending the meeting should be aware that we cannot guarantee that they will not be filmed or recorded by others attending the meeting. Members of the public participating in the meeting (e.g. making deputations, asking questions, making oral protests) should be aware that they are likely to be filmed, recorded or reported on. By entering the meeting room and using the public seating area, you are consenting to being filmed and to the possible use of those images and sound recordings.

The Chair of the meeting has the discretion to terminate or suspend filming or recording, if in his or her opinion continuation of the filming, recording or reporting would disrupt or prejudice the proceedings, infringe the rights of any individual, or may lead to the breach of a legal obligation by the Council.

2. PLANNING PROTOCOL

The Planning Committee abides by the Council's Planning Protocol 2017. A factsheet covering some of the key points within the protocol as well as some of the context for Haringey's planning process is provided alongside the agenda pack available to the public at each meeting as well as on the Haringey Planning Committee webpage.

The planning system manages the use and development of land and buildings. The overall aim of the system is to ensure a balance between enabling development to take place and conserving and protecting the environment and local amenities. Planning can also help tackle climate change and overall seeks to create better public places for people to live, work and play. It is important that the public understand that the committee makes planning decisions in this context. These decisions are rarely simple and often involve balancing competing priorities. Councillors and officers

have a duty to ensure that the public are consulted, involved and where possible, understand the decisions being made.

Neither the number of objectors or supporters nor the extent of their opposition or support are of themselves material planning considerations.

The Planning Committee is held as a meeting in public and not a public meeting. The right to speak from the floor is agreed beforehand in consultation with officers and the Chair. Any interruptions from the public may mean that the Chamber needs to be cleared.

3. APOLOGIES

4. URGENT BUSINESS

The Chair will consider the admission of any late items of urgent business. Late items will be considered under the agenda item where they appear. New items will be dealt with at item 12 below.

5. DECLARATIONS OF INTEREST

A member with a disclosable pecuniary interest or a prejudicial interest in a matter who attends a meeting of the authority at which the matter is considered:

- (i) must disclose the interest at the start of the meeting or when the interest becomes apparent, and
- (ii) may not participate in any discussion or vote on the matter and must withdraw from the meeting room.

A member who discloses at a meeting a disclosable pecuniary interest which is not registered in the Register of Members' Interests or the subject of a pending notification must notify the Monitoring Officer of the interest within 28 days of the disclosure.

Disclosable pecuniary interests, personal interests and prejudicial interests are defined at Paragraphs 5-7 and Appendix A of the Members' Code of Conduct

6. MINUTES (PAGES 1 - 6)

To confirm and sign the minutes of the Planning Sub Committee held on 11 January 2021.

7. PLANNING APPLICATIONS

In accordance with the Sub Committee's protocol for hearing representations; when the recommendation is to grant planning permission, two objectors may be given up to 6 minutes (divided between them) to make representations. Where the recommendation is to refuse planning permission, the applicant

and supporters will be allowed to address the Committee. For items considered previously by the Committee and deferred, where the recommendation is to grant permission, one objector may be given up to 3 minutes to make representations.

8. HGY/2020/2794 - LAND TO THE NORTH OF ERMINE ROAD N15 (PAGES 7 - 126)

Proposal: Temporary planning permission for a period of 7 years to provide 38 modular units for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.

Recommendation: GRANT

9. HGY/2020/3036 - REAR OF 132 STATION ROAD, N22 7SX (PAGES 127 - 200)

Proposal: Construction of 6 dwellings set in landscaped area and creation of 'community wildlife garden' following the demolition of existing structures.

Recommendation: GRANT

10. UPDATE ON MAJOR PROPOSALS (PAGES 201 - 214)

To advise of major proposals in the pipeline including those awaiting the issue of the decision notice following a committee resolution and subsequent signature of the section 106 agreement; applications submitted and awaiting determination; and proposals being discussed at the pre-application stage.

11. APPLICATIONS DETERMINED UNDER DELEGATED POWERS (PAGES 215 - 246)

To advise the Planning Committee of decisions on planning applications taken under delegated powers for the period 28.12.20-22.01.21.

12. NEW ITEMS OF URGENT BUSINESS

To consider any items admitted at item 4 above.

13. DATE OF NEXT MEETING

Felicity Foley, Committees Manager
Tel – 020 8489 2919
Fax – 020 8881 5218
Email: felicity.foley@haringey.gov.uk

John Jones
Monitoring Officer (Interim)
River Park House, 225 High Road, Wood Green, N22 8HQ
Friday, 29 January 2021

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MINUTES OF MEETING Planning Sub Committee HELD ON Monday, 11th January, 2021, 7.00 - 10.00 pm

PRESENT:

Councillors: Sarah Williams (Chair), John Bevan, Luke Cawley-Harrison, Peter Mitchell, Reg Rice, Viv Ross, Yvonne Say, Liz Morris and Sheila Peacock

485. FILMING AT MEETINGS

The Chair advised that the meeting would be streamed live on the Council's website.

486. APOLOGIES

Apologies for absence were received from Councillors Adamou, Basu, Hinchcliffe and Stone.

Councillor Morris was in attendance for Councillor Hinchcliffe.

Councillor Peacock was in attendance for Councillor Stone.

487. URGENT BUSINESS

There was no such business.

488. DECLARATIONS OF INTEREST

None.

Councillor Bevan advised that he was a member of the Homes for Haringey Board, but that this would not affect his consideration of the application (Ermine Road).

489. MINUTES

RESOLVED that the minutes of the meeting held on 7 December 2020 be approved as a correct record.

490. HGY/2020/2794 - LAND TO THE NORTH OF ERMINE ROAD N15

The Committee considered an application for the temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.

Officers responded to questions from the Committee:

- The units were 24m² which was below the minimum threshold for a one bedroom flat. This however was a specific proposal and was not designed to be the same

size as a standard one bedroom flat. It was designed to house a single person and to help people move on with their lives after a period of homelessness.

- The stairwells were to provide access to the units and not for occupants to gather as seating areas.
- The build time of the units was around one week, however the landscaping would take longer.
- Condition 18 required further surveys to be carried out to ensure that if a culverted main river was found to be present under or within 4m of the site, then a condition survey and strategy was required to protect the culvert before any further development can be carried out.
- The site would be managed by on-site staff, who could deal with any issues which may arise.
- A condition had been added to recommend a carbon offsetting payment.
- There had been no objections made by carbon management in relation to the installation of the air source heat pumps.
- There was private amenity space around the blocks on the grassed areas, but no communal spaces within the units.
- If the Committee felt that there was a specific issue with air source heat pumps and noise, then a condition could be added to ensure that the noise was at an acceptable domestic level.
- There was a 1.8m fence at the back of the site, and a 1.5m railing at the front.

Laura Budka spoke in objection to the application. There were concerns around the loss of privacy for neighbouring properties. The units would be two storeys high which would mean that neighbouring houses and gardens would be overlooked. Although the units did not have balconies, the access platforms were in direct eyelines of bedrooms. Ms Budka felt that local children would be discouraged from playing outside. No information had been provided on the residents who would be living in the units, and it was concerning to think that people with drug and alcohol issues would be housed closely to young families. The application stated that extra CCTV would be installed to mitigate the impact of the unit, but it was unacceptable that residents would experience any negative impacts due to the development. It was unclear how the development would make any positive impact on the area.

Maria Lincoln spoke in objection to the application. The units were 35% smaller than the national space standards, with exposed stairwells and frontages, causing a detrimental effect to the visual amenity on Ermine Road. There seemed to be no evidence of a noise impact study or the likely increase of anti-social behaviour. Residents of Ermine Road already experienced issues with littering, loitering and other anti-social behaviours, and Ms Lincoln stated that this was not the right area to build a development for vulnerable people to help them integrate back into a community.

Simche Steinberger, Hackney Council Ward Councillor, spoke in objection to the application. He was concerned that he had not been consulted on the application, given that he was a Councillor in a neighbouring ward. Mr Steinberger felt that the units were too small to be considered suitable. He felt that it would be unfair to local residents to have such a development on their road. He reiterated his worry that there

had been no consultation with Hackney Council and requested that the application be refused.

Councillor Barbara Blake spoke in objection to the application. She advised that majority of the residents in Ermine Road were opposed to the development. Haringey Council had not provided a strong management plan for the development, and there were concerns that the development would not be managed 24/7 and that the evenings would have a concierge on-site rather than site management. The development would have a detrimental impact on neighbours, with overlooking, increasing anti-social behaviour and noise levels. There was no plan to deal with residents of the development congregating outside of the units. The colour of the units was also considered to be an eyesore. Councillor Blake requested that the Committee take the views of residents into consideration and refuse the application.

Councillor Erdal Dogan spoke in objection to the application. Ermine Road and Seven Sisters Road were hotspots for crime and anti-social behaviour, with cheap alcohol sold everywhere. This did not make the area suitable for the type of residents in the development. If the applicant was granted, anti-social behaviour would become more acute in the area. Councillor Dogan added that residents should not have to suffer from disturbed sleep, anxiety and feeling unsafe in their own homes.

Objectors responded to questions from the Committee:

- Letters from the Council had been sent to residents in October. Consultation meetings had been cancelled and rescheduled to take place 2 days before the end of the statutory consultation period. It was felt that the person chairing the consultation meeting was in support of the application so residents had not felt comfortable putting their objections forward.
- Residents had not been informed of the type of support residents of the development would require. It was only when the job adverts for the units had been advertised that residents were aware that the job role required people who had experience dealing with alcohol and drug issues, and the criminal justice system. It was felt that there was a lack of transparency by the Council.

The Applicant Party - Mark Sleigh (Planning Agent), Emma Fletcher (Applicant – Hill), Gill Taylor (LBH), Robbie Erbmann (LBH) and Jon Glackin (Streetskitchen) - addressed the Committee.

The Covid-19 pandemic had resulted in unprecedented pressures on homelessness. The development would provide 37 essential homes for homeless people in the borough. Each unit would be provided with everything required for a home, and each unit met secure by design principles. The corporate colours for Hill were orange and grey and the modular units were manufactured in these colours.

The Council had housed over 850 homeless people during the pandemic, with people still living in hotels while waiting for longer term options. The units would provide support to people to move forward with their lives following a period of homelessness. The development would be funded through the rough sleepers accommodation programme, a scheme which was recognised by the MHCLG, who were looking to develop housing quickly for people in need. The borough currently provided more than 500 supported housing units in the borough and had experience in delivering

support housing services. Service users were usually more likely to be victims of crime rather than perpetrators, however the service worked closely with local Safer Neighbourhood Teams and had robust anti-social behaviour plans.

Each unit would house one person, over the age of 18. The tenancies would be short-hold, for up to two years. There would be a referrals process before any tenants were accepted. There would be a minimum of two staff on site at all times, and each resident will have their own key worker.

The units were a high quality design and the application was a product of extensive collaboration with Haringey Council officers.

The applicant team responded along with officers to questions from the Committee:

- There would be a member of staff in the office at all times, other staff would be encouraged to be working with residents within the units.
- The units were not designed to be hostels, they were designed to be independent tenancies so no communal areas would be provided. Each resident would be encouraged to gain their independence and have their own front doors. Although only one person would be allowed to live in the unit, there was space for visitors.
- The air source heat pumps were not located within the units, but within onsite plant rooms – one pump could provide heat to six units.
- It was appreciated that the colour of the units was not to everybody's tastes, however these were the colours that the units were manufactured in. 16 of the units would be gifted by Hill. Any changes to the colour would result in a delay in delivering the units to the site as this would have a significant impact on the building process.
- Hackney Council had not been consulted with as the distance from the site was over 450m.

Councillor Peacock moved that the based on the concerns of residents, Councillor Barbara Blake and Councillor Dogan, that block A be removed from the application and deferred to a future meeting. Councillor Bevan seconded the motion and added the issues raised with the colour and design of the units in that they did not complement the adjacent estate.

Dean Hermitage advised that any application had to be deferred as a whole.

The Chair moved that the application be deferred pending the removal of block A and design improved, and following a vote with 5 in favour and 3 against it was

RESOLVED that the application be deferred.

491. PPA/2020/0024 - WEST INDIAN CULTURAL CENTRE (TO NOW BE KNOWN AS AFRICAN CARIBBEAN CULTURAL CENTRE) SITE, CLARENDON ROAD OFF HORNSEY PARK ROAD, N8 0DD

The Planning Officer and Applicant Team outlined the proposal for the demolition of the three existing buildings and construction of a part 12/14 and part 6/8/10 storey

building incorporating a two storey base to provide a new cultural centre, co-workspace, gym and cafe, 85 residential units and 140 co-living units, with access, public realm improvements and landscaping and car and cycle parking.

The Applicant Team responded to questions from the Committee:

- The internal amenity space per person was 6.4m².
- The building would be set back to respect existing trees, and substantial section 278 improvements would be made.
- There would be two entrances for the buildings, which would access properties of all tenures.
- Units on the corners of the building would be dual aspect. Single aspect units were all East or West facing to ensure the best sunlight.
- The cultural centre was operated on land owned by the Council, and held a 95 year lease. Ownership was irrelevant when considering planning permission.

492. UPDATE ON MAJOR PROPOSALS

The Chair asked Members to send any queries by email to Dean Hermitage.

493. APPLICATIONS DETERMINED UNDER DELEGATED POWERS

The Chair asked Members to send any queries by email to Dean Hermitage.

494. NEW ITEMS OF URGENT BUSINESS

None.

495. DATE OF NEXT MEETING

8 February 2021

CHAIR: Councillor Sarah Williams

Signed by Chair

Date

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Planning Sub Committee

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS

Reference No: HGY/2020/2794

Ward: Seven Sisters

Address: Land to the North of Ermine Road N15

Proposal: Temporary planning permission for a period of 7 years to provide 38 modular units for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.

Applicant: Hill Residential

Ownership: Council

Case Officer Contact: Philip Elliott

Site Visit Date: N/A

Date received: 06/11/2020 **Last amended date:** 19/01/2021.

- 1.1 This application was initially reported to Planning Sub-Committee on 11 January 2021 as it is a major application on Council owned land, and significant material planning objections were received during the first consultation process.
- 1.2 A decision on this application was deferred at the Planning Sub-Committee meeting on the 11 January 2021. This was to allow for further information / amendments to be submitted by the applicant to respond to concerns raised by members of the committee in relation to the character of development and its impact on the privacy and amenities of existing residents.
- 1.3 Amendments were received on 19 January 2021. These are summarised as:
 - Addition of louvred screens to front of Block A (upper level)
 - Changes to the colour of the units
- 1.4 Interested parties were notified of the amendments on 19 January 2021. All responses to both consultations remain relevant and are material considerations (see original report to Committee appended at Appendix 4).

1.5. SUMMARY OF KEY REASONS FOR RECOMMENDATION

- There is a significant need for accommodation for rough sleepers in Haringey, and this proposal provides 37 units of temporary accommodation specifically designed to meet the needs of people who have experienced homelessness. It would improve the life expectancy of residents and reduce the financial costs of temporary accommodation to the Council.
- The proposal would provide much needed temporary accommodation for 37 Haringey residents that have experienced homelessness;
- The modular construction enables the proposed homes to be built quickly so that interventions can be made as soon as possible to help those in need;
- The proposal would make a positive contribution to Ermine Road, improving the character of this vacant site.
- The proposed development would preserve the setting of the nearby Seven Sisters/Page Green Conservation Area and would have an acceptable appearance from within the Conservation Area and the locally listed Dutch House.
- There would be no material impact on parking in the area.
- Impacts on nearby properties would be suitably mitigated, particularly given the revisions which minimise overlooking and safeguard privacy.

2. RECOMMENDATION

- 2.1 That the Committee resolve to GRANT planning permission and that the Head of Development Management or Assistant Director is authorised to issue the planning permission and impose conditions and informatives.

Conditions (the full text of recommended conditions is contained in Appendix 1)

Presumption in Favour of Sustainable Development

- In the event that members choose to make a decision contrary to the officer recommendation (that the proposed development accords with the development plan overall), it will be necessary to consider the presumption in favour of sustainable development in the NPPF. This is because the Council's delivery of housing over the last three years is substantially below its housing target and so paragraph 11(d) of the NPPF is engaged by virtue of footnote 7 of the NPPF. Members must state their reasons including why it is considered that the presumption is not engaged.

CONTENTS

- 3) KEY AMENDMENTS SINCE DEFERRAL
- 4) FURTHER CONSULTATION & RESPONSES
- 5) LOCAL REPRESENTATIONS
- 6) ASSESSMENT OF THE AMENDMENTS
- 7) RECOMMENDATION
- 8) PLANNING CONDITIONS & INFORMATIVES

APPENDICES:

Appendix 1	Planning Conditions and Informatives
Appendix 2	Plans and Images
Appendix 3	Addendum to Report for Committee 11 January 2021
Appendix 4	Committee Report for 11 January 2021

3.0 KEY AMENDMENTS SINCE DEFERRAL

3.1 Amendments:

3.1.1. Concerns were raised by members of the committee at the meeting on 11 January 2021 in relation to the potential for overlooking from the stair and raised platform to Block A (which provides access to the upper storey of this block) towards existing dwellings on Ermine Road. Concerns were also raised about the colour of the front and rear elevations of the modular units which were proposed to be in orange.

3.1.2. In response to this concern the applicants have submitted revised drawings that:

- **Incorporate louvred screens** to the upper part of both staircases and to the ends of the raised platform to Block A. These screens would restrict views southwards of properties on Ermine Road from the upper storey of Block A. This increases the closest, non-screened window to window distance to from approximately 13 metres to 15 metres between Block A and the nearest property at number 56 Ermine Road.
- **Grey painted finish to match the flank elevations. The front doors would remain orange.** This would significantly reduce the presence of orange in the development.

4. CONSULTATION & RESPONSES

4.1. **Application Consultation (all original responses remain relevant and this report should be read alongside the original report in appendix 4, which sets-out responses to the original consultation.)**

4.1.1. The following were consulted on the amended application:

Internal:

1) Design

- *The louvred screens would hide pedestrians nearing the top of the stairs and at those corners from the nearest existing houses and reduce their visibility from the street, whilst still allowing some visibility of them in the centre of the access balcony and still leaving the front doors and windows to those 1st floor flats visible from the street and with a view of the street at the T-junction, where there will be no close direct line of site from those flats or their balcony to existing residential neighbours.*
- *However, it is considered important for the visibility of the development, and to integrate the residents into the community, that the flats in Block A look out*

into the street, their front doors and living room windows are visible from the street and benefit from a view of the street.

- *The louvred screens will only improve views of the development, by reducing the visible number of stairs and balconies, which can make the elevational composition more cluttered.*
- *The change in colours will make the proposal look less “strident”, with less bright orange and greyer in the external colour scheme. However, as each entrance door will still be orange, it will retain that counterpoint of stronger, more cheerful colour, and they will emphasise the location of each entrance door and the individual identity of each flat. The CGIs show the colour changes reduce the impact of the proposals, visually recessing them into the landscape and context.*

External:

2) Environment Agency (EA)

- *No further comments received at time of drafting report. Responses will be reported via an Addendum Report on 1 February 2021.*

3) London Fire Brigade (LFB)

- *No further comments received at time of drafting report. Responses will be reported via an Addendum Report on 1 February 2021.*

4) Designing Out Crime Officer - Metropolitan Police Service

- *No comments received at time of drafting report. Responses will be reported via an Addendum Report on 1 February 2021.*

All previous responses are set out in the original report in Appendix 4.

5. LOCAL REPRESENTATIONS

5.1 The following were consulted on the amendments:

- All residents that commented on the original scheme [66] were notified of the amended plans.

*It is noted that several commentators did not provide an email address or a full postal address when submitting their representations. The Council has contacted as many people as possible given the information available.

5.2 The number of representations received from neighbours, local groups etc in response to notification and publicity of the application were as follows:

- Number of individual responses to date:

- 124 instances of objection have been received (59 were reported on 11th Jan committee report not including the petition).
- 90 instances of support have been received (1 was reported on 11th Jan committee report) .
- The above figures include names and addresses added to a petition objecting to the proposal. 14 of individuals listed on the petition also sent in objections so they have only been counted once.

➤ The additional letters of support raise the following new points:

- the high rates of homelessness and rough sleeping in Haringey and the UK,
- that providing homes to help resolve this problem and support vulnerable members of the community is a good idea – particularly during a pandemic,
- that the management and support for the proposed residents would be holistic and appropriate to mitigate concerns about anti-social behaviour; and
- that the design would complement and add vibrancy to the local area and built environment.

➤ The letters of objection relate to concerns highlighted in the original report (Appendix 4) relating to anti-social behaviour, noise & disturbance, impacts on neighbouring amenity (including overlooking) & parking, the design, and the size and number of units/homes.

- The consultation period will expire on the 29 January. Any additional responses received after this report was drafted will be reported via an Addendum Report on 8 February 2021.

5.3The following local groups/societies made representations:

- No representations received from local groups/societies to date.

5.4The following Councillor made representations:

- Councillor Barbara Blake commented on the original application (please see Appendix 4).

6 ASSESSMENT OF THE AMENDMENTS

- Officers considered the original scheme complied with the relevant policies (detailed in the report at Appendix 4). The amendments seek to address the concerns raised by Members and improve both the design of the

building as well as its impact on neighbouring properties. Conditions are recommended that would ensure the amendments are delivered and retained in perpetuity. The following paragraphs will assess the amendments in detail.

Impact on the amenity of neighbours

- 6.1.1 The policies relevant to the impact of the development on the amenity of adjoining occupiers and residents are set out in section 6.3 of the original report. The following paragraphs will assess the amendments and the impact they have in this regard.
- 6.1.2 The louvred screening would prevent direct views from the upper storey of Block A towards number 56 Ermine Road and the closest terraced property to the southwest. The screening to the western elevation would wrap around the corner of the building which would be closest to number 56 and the other properties in the terrace.
- 6.1.3 The screening would minimise any overlooking of number 56 and the wider terrace. The screening would taper down to the east as it moves beyond the front door of the first unit. Beyond this point, Block A begins to angle away from number 56 and views of this property and the wider terrace become more oblique.
- 6.1.4 The distance between number 56 and the platform which would now be screened off is 10 – 11m. The screening would increase the window to window distance to approx. 15m. It would also reduce the perception of overlooking towards and from the development by screening off the stair access.
- 6.1.5 The screening has not been continued along the entire platform. Views from vantage points beyond the screening would not result in harmful overlooking, given the angle and distances involved.
- 6.1.6 An unrelenting fence at this level would also be an overbearing feature on the overall design and would create an undue sense of enclosure for occupiers of all of the properties at this level.
- 6.1.7 The screening to the eastern elevation would block views towards numbers 57 and 58 where Block A would be closest to the terrace those properties form a part of. These properties are orientated so that the rear elevations face west and, therefore, overlooking is constricted in any event. The screening ensures that any overlooking is minimised and will be secured by condition (No.20).
- 6.1.8 The louvred design of the screening allows light to come through and provides a degree of perforation to provide design interest while avoiding solid expanses of steel.

Design

- 6.1.9 The policies relevant to the design of the development are set out in section 6.5 of the original report. The following paragraphs will assess the amendments and the impact they have in this regard.
- 6.1.10 Members raised concerns in the January meeting that the orange colour would be obtrusive and would not relate positively to neighbouring structures. The front and rear elevation panels of the modular units have been amended to a grey colour (RAL7043) to match the window frames to the units. The doors to the units would remain in orange. The colours can be secured by condition (No.19).
- 6.1.11 The current proposal significantly reduces the amount of orange but retains some colour to provide a degree of vibrancy amongst the grey of the rest of the development. The rear elevation of Block C, which is visible from the High Road and the nearby conservation area, would not contain any orange.
- 6.1.12 The revised colouration is a more sympathetic approach that relates positively to neighbouring structures and, along with the landscaping, would make a positive contribution to the site, improving the character and quality of the area with well-considered and proportioned buildings that provide much needed specialist accommodation.

Housing Delivery Test

- 6.1.13 The 2020 Housing Delivery Test (HDT) results were published on 19 January 2021 and as a result the LPA is now subject to the 'presumption in favour of sustainable development' and paragraph 11d of the NPPF is relevant. The Council's delivery of housing over the last three years is substantially below its housing target and so paragraph 11d) of the NPPF is engaged by virtue of footnote 7 of the NPPF. Nevertheless, the proposed development has been found to be in accordance with development plan policies and therefore consideration of para 11(d) is not required in this instance (but would be, if the application was to be refused).

Other Issues

- 6.1.14 All other issues are discussed in the original report at Appendix 4.

6.2 Conclusion

- 6.2.1 The amendments made since deferral provide for an improved relationship with existing properties by way of limiting overlooking from the upper level of Block A, and the external colouring provides a 'visually quieter' addition to the street scene than that originally proposed. This is considered an appropriate design response in the context of the wider planning assessment as set out in the original report.

6.2.2 This report and recommendation should be read alongside the original report to committee (Appendix 4).

6.2.3 Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION.

6.3 CIL

6.3.1 Based on the information given on the plans, the Mayoral CIL charge will be £54,720 (912sqm x £60.55) and the Haringey CIL charge will be £19,061 (912sqm x £20.90). This would be charged in accordance with the CIL Charging Schedule and any eligible relief and includes indexation in line with the RICS CIL Index.

7 RECOMMENDATION

GRANT PERMISSION subject to conditions in Appendix 1

Applicant's drawing No.(s)

Location Plan 109-08-PS-001; Proposed Site Plan 109-08-PS-002 K; Proposed Ground Floor Plan 109-08-PS-010 K; Proposed First Floor Plan 109-08-PS-011; Block A Elevations / Street Section 109-08-PS-043 Rev. B; Block B Elevations / Site Section A 109-08-PS-044 Rev: A; Block C Elevations / Site Section B 109-08-PS-045 Rev: A; ROOF PLAN CHM-D-18 D04; GENERAL PLAN CHM-D-01 D05.

Appendix 1

Subject to the following condition(s)

Temporary Permission

- 1) This permission shall be for a limited period expiring on 12/02/2028 when the building hereby approved shall be removed and the land reinstated.

Reason: The permanent retention of the building may prejudice the future development of the site thus preventing the optimal use of the site inconsistent with Policy 3.4 of the London Plan 2016.

Approved Plans

- 2) The approved plans comprise drawing numbers (Location Plan 109-08-PS-001; Proposed Site Plan 109-08-PS-002 K; Proposed Ground Floor Plan 109-08-PS-010 K; Proposed First Floor Plan 109-08-PS-011; Block A Elevations / Street Section 109-08-PS-043 Rev. B; Block B Elevations / Site Section A 109-08-PS-044 Rev: A; Block C Elevations / Site Section B 109-08-PS-045 Rev: A; ROOF PLAN CHM-D-18 D04; GENERAL PLAN CHM-D-01 D05). The development shall be completed in accordance with the approved plans and retained as such thereafter for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: In order to ensure the development is carried out in accordance with the approved details and in the interests of amenity and good planning.

Materials as indicated on approved plans

- 3) The external materials to be used for the proposed development shall match the colour, size, shape, and texture of the materials indicated on the approved plans except where conditions attached to this planning permission indicate otherwise.

Reason: In order to ensure a satisfactory appearance for the proposed development, to safeguard the visual amenity of neighbouring properties and the appearance of the locality consistent with Policy 7.6 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2013 and Policy DM1 of The Development Management DPD 2017.

No Parking Permits for New Residents

- 4) The development shall not be occupied other than through a lease/license/agreement between the operator and each resident that ensures

that occupants of the development hereby approved are obligated not to apply for a parking permit for any Controlled Parking Zone (CPZ).

Reason: In order to ensure car parking is restricted in line with levels of existing and future public transport accessibility and connectivity and to comply with Policy T6 of the Publication London Plan (2020) & Policy DM32 of The Development Management DPD 2017.

All Parking Delivered as Approved

- 5) The vehicular and cycle parking spaces shown on the approved plans shall be laid out and installed as approved and retained for the lifetime of the development. The hatched parking space indicated on drawing '109-08-PS-010 K' shall only be used for deliveries & servicing; or in/for emergencies except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: In order to ensure sufficient space is given over within the development to facilitate safe, clean, and efficient deliveries and servicing and to comply with Policies T5, T6, T6.1, & T7 of the Publication London Plan (2020).

Construction Management/Logistics Plan

- 6) Prior to above ground works, a Construction Management/Logistics Plan shall be submitted to and approved in writing by the local planning authority. The CLP shall be in accordance with Transport for London's Construction Logistics Plan Guidance (July 2017) and shall provide details on:
- i. Details of key phases of the construction programme, corresponding site layout, number of staff including modes of travel, parking provision (vehicles and cycles);
 - ii. Arrangements for management of construction material deliveries / removal, material storage, skip storage;
 - iii. Daily number and size of construction vehicles expected, vehicular swept paths (with 300mm error margins) to demonstrate largest construction vehicles arriving, parking in loading/unloading area, manoeuvring and departing in forward gear;
 - iv. Details of all temporary traffic management and parking restrictions required;
 - v. Details of any highway licences required due to the crane oversailing the public highway;
 - vi. Wheel washing facilities to keep highway clean of mud etc;
 - vii. Arrangements for management of Health and safety;
 - viii. Arrangements for dealing with complaints;
 - ix. Hours of operations;

- x. Monitoring and joint working arrangements, where appropriate;
- xi. Site access and car parking arrangements;
- xii. Delivery booking systems (allocated delivery slots for site management);
- xiii. A swept path analysis for crane vehicles carrying the necessary equipment;
- xiv. Agreed routes to/from the site;
- xv. Confirmation that all vehicles are recognised in the Fleet Operators Recognition Scheme (FORS) or similar;
- xvi. Timing of deliveries to and removals from the site by construction vehicles (to avoid peak times, as agreed with Highways Authority, 07.00 to 9.00 and 16.00 to 18.00, where possible); and
- xvii. Travel plans for staff/personnel involved in construction works to detail the measures to encourage sustainable travel to the site during the construction phase; and
- xviii. Joint arrangements with neighbouring developers for staff parking, Lorry Parking, and consolidation of facilities such as concrete batching; and
- xix. Temporary obstructions during the construction and delivery must be kept to a minimum and should not encroach on the clear space needed to provide safe passage for pedestrians or obstruct the flow of traffic on the TLRN.

Reason: In order to ensure that the proposal is consistent with Publication London Plan Policy T7 and to safeguard residential amenity, reduce congestion and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality and to comply with DMDPD Policy DM23: Environmental Protection.

Energy Strategy

- 7) The development shall be constructed in accordance with:
- the Carbon Reduction Statement – ModulHaus Ermine Road prepared by Volumetric Modular Ltd (dated December 2020);
 - Overheating, MVHR, ASHP Technical Information Report prepared by Hill (dated 5 October 2020); and
 - the TM59 Overheating Study prepared by The Richards Design Partnership (dated November 2020).

and

the energy efficient materials and air source heat pumps shall be maintained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: To ensure the development can comply with the Energy Hierarchy in line with London Plan 2016 Policy 5.2, Publication London Plan Policy SI 2 and Local Plan Policy SP4.

Carbon Offset Contribution

- 8) The development hereby approved shall not be occupied until a payment of £7,454.65 towards carbon reduction measures in Haringey has been paid to the Council's Carbon Management Team to bring the level of carbon reduction from the site in line with the London Plan Policies 5.1, 5.2, 5.3, 5.7, 5.8, 5.9, 5.10, and 5.11; The Publication London Plan (2020) Policy SI 2; and Local Plan Policy SP4.

Reason: To ensure the development can comply with Publication London Plan Policy SI2 and Local Plan Policy SP4.

Land Contamination

- 9) Prior to any further work on site:
- a) Using the information already acquired from the submitted Phase I Geo-Environmental Assessment with reference DS-21906G-20-420 proposed by IDOM Merebrook Limited dated October 2020, a site investigation shall be designed for the site using information obtained from the desktop study and Conceptual Model. The site investigation must be comprehensive enough to enable; a risk assessment to be undertaken, refinement of the Conceptual Model, and the development of a Method Statement detailing the remediation requirements.
 - b) The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority which shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site.
 - c) Where remediation of contamination on the site is required, completion of the remediation detailed in the method statement shall be carried out and;
 - d) A report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority before the development is occupied.

Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety and to comply with London Plan (2016) policy 5.21 (Contaminated land) and DMDPD Policy DM23.

Unexpected Contamination

- 10) If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until a remediation strategy detailing how this contamination will be dealt with has been submitted to and

approved in writing by the Local Planning Authority. The remediation strategy shall be implemented as approved.

Reason: To ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels of water pollution from previously unidentified contamination sources at the development site in line with paragraph 109 of the National Planning Policy Framework.

CEMP

- 11) Prior to above ground works, a Construction Environmental Management Plan (CEMP) shall be submitted to and approved in writing by the local planning authority.

The following applies to above:

- a) The CEMP shall include an Air Quality and Dust Management Plan (AQDMP).
- b) The CEMP shall provide details of how construction works are to be undertaken respectively and shall include:
 - i. A construction method statement which identifies the stages and details how works will be undertaken;
 - ii. Details of working hours, which unless otherwise agreed with the Local Planning Authority shall be limited to 08.00 to 18.00 Monday to Friday and 08.00 to 13.00 on Saturdays;
 - iii. Details of plant and machinery to be used during demolition/construction works;
 - iv. Details of an Unexploded Ordnance Survey;
 - v. Details of the waste management strategy;
 - vi. Details of community engagement arrangements;
 - vii. Details of any acoustic hoarding;
 - viii. A temporary drainage strategy and performance specification to control surface water runoff and Pollution Prevention Plan (in accordance with Environment Agency guidance);
 - ix. Details of external lighting; and,
 - x. Details of any other standard environmental management and control measures to be implemented.
- c) The AQDMP will be in accordance with the Greater London Authority SPG Dust and Emissions Control (2014) and shall include:
 - i. Mitigation measures to manage and minimise demolition/construction dust emissions during works;
 - ii. Details confirming the Plot has been registered at <http://nrmm.london> where applicable;

- iii. Evidence of Non-Road Mobile Machinery (NRMM) and plant registration shall be available on site in the event of Local Authority Inspection;
- iv. An inventory of NRMM currently on site (machinery should be regularly serviced, and service logs kept on site, which includes proof of emission limits for equipment for inspection);
- v. A Dust Risk Assessment for the works; and
- vi. Lorry Parking, in joint arrangement where appropriate.

Reason: To safeguard residential amenity, reduce congestion and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality and to comply with Policy 5.21 (Contaminated land) of the London Plan (2016) and DMDPD Policy DM23: Environmental Protection.

Drainage / Flooding (FRA)

- 12) Prior to above ground works, a revised FRA shall be submitted to and approved in writing by the local planning authority. The revised FRA shall provide new rainfall data using Flood Estimation Handbook (FEH) rainfall theory, in accordance with Haringey guidance.

The FRA shall include a completed London Sustainable Drainage pro-forma, as well as evidence that Thames Water have given consent to connect to their network and capacity exists to receive the surface water.

Reason: In order to comply with Policies DM24, 5, 6, & 9 of the Haringey Development Management DPD (2017), policy SP5 of the Local Plan.

Provision & Retention of SuDS

- 13) Prior to above ground works, Section 4 (Sustainable Drainage Maintenance Strategy) & Appendix B (Proposed Drainage Strategy Drawings) of 'Design Note – Ermine Rd Drainage & Flood Risk 16/12/2020' and the SuDS options it proposes (namely Below Ground Cellular Attenuation & Permeable Paving) shall be provided, maintained in accordance with the associated Maintenance Schedules, and retained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

For the avoidance of doubt, all hardstanding areas shall be permeable.

Reason: In order to ensure a satisfactory provision for drainage on site and ensure suitable drainage provision for the development and comply with Policies DM24,

5, & 6 of the Haringey Development Management DPD (2017), policy SP5 of the Local Plan.

No Light Spill into SINC (Bats)

- 14) Light from external lighting in the development shall not spill to a harmful extent into the adjacent ecological asset (Tottenham Railsides SINC). Any lighting located near the northwest boundary of the site shall have directional shading to ensure that light spillage into the SINC does not have an undue impact on foraging bats and their habitats.

Reason: In order to ensure bats and their habitats are suitably protected and comply with Policies DM19 (Nature Conservation) of the Haringey Development Management DPD (2017), policy 7.19 of the London Plan, and Policy G6 of the Publication London Plan.

Tree protection (SINC/Bats)

- 15) The existing trees adjacent to the northwest boundary of the site shall not be lopped, felled or otherwise affected in any way (including raising and lowering soil levels under the crown spread of the trees) and no excavation shall be cut under the crown spread of the trees without the prior written permission of the Local Planning Authority.

Reason: In order to ensure bats and their habitats are suitably protected and to safeguard trees in the interest of visual amenity of the area consistent with Policy 7.21 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2017 and Policies DM1 and DM19 of The Development Management DPD (2017).

Waste storage delivered and made accessible

- 16) The waste storage shown on the approved plans shall be delivered and retained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

On occupation of the development - the codes, keys, transponders or any other type of access equipment to the waste store shall be provided to the Council and access to the waste store shall remain unobstructed and within 10m from Ermine Road on collection day.

Reason: In order to ensure waste storage is suitably accessible and to comply with Policy DM4 of The Development Management DPD (2017).

Secured by Design

- 17) Prior to occupation of the development, details of full 'Secured by Design' Accreditation shall be submitted to and approved in writing by the Local Planning Authority. The details shall demonstrate consultation with the Metropolitan Police Designing Out Crime Officers. The development shall be carried out in accordance with the approved details and maintained thereafter for the lifetime of the development.

Reason: To ensure safe and secure development and reduce crime and to comply with Publication London Plan Policy D11 and DMDPD Policy DM2.

Culvert Protection

- 18) Prior to any above ground works, a ground penetrating radar (GPR) survey of the site and within 4m of the northern boundary shall be carried out to ascertain if the Stonebridge Brook (culvert) is located under or near to the site.

If the GPR survey finds the culvert under, or within 4m of, the site then no further development shall be carried out until a condition survey has been completed and a strategy put in place to protect the culvert. The strategy shall be submitted to and approved in writing by, the local planning authority (in consultation with the Environment Agency), and shall be implemented as approved. The strategy shall include the following components;

- Details of the location, depth, and structural condition of the culvert.
- Plans to repair any damage the culvert to ensure structural stability for the lifetime of the development.
- Details of how work would be carried out on site without damaging the structure, or impeding the function of, the culvert (including loading calculations).

If the GPR survey does not locate the culvert, then the results shall be shared with the EA and works can proceed in accordance with the other conditions of the permission.

Reason: To reduce the risk of flooding to the proposed development and future occupants, ensure the structural integrity of the existing flood defences and reduce the risk of flooding from blockages to any existing culvert in accordance with Policy DM28: Protecting and Enhancing Watercourses and Flood Defences.

Building colour

- 19) The external elevations to be used for the proposed modular units shall be a grey colour, with an orange front door and retained as such for the lifetime of the development unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure a satisfactory appearance for the proposed development, to safeguard the visual amenity of neighbouring properties and the

appearance of the locality consistent with Policy 7.6 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2013 and Policy DM1 of The Development Management DPD 2017.

Louvred screening erected and retained

- 20) The louvred screening indicated on the approved plans shall be installed as shown on drawing number '109-08-PS-043 Rev. B' prior to occupation of Block A and retained for the lifetime of the development.

Reason: In order to ensure a satisfactory appearance for the proposed development, to safeguard the visual amenity of neighbouring properties and the appearance of the locality consistent with Policy 7.6 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2013 and Policy DM1 of The Development Management DPD 2017.

Informatives:

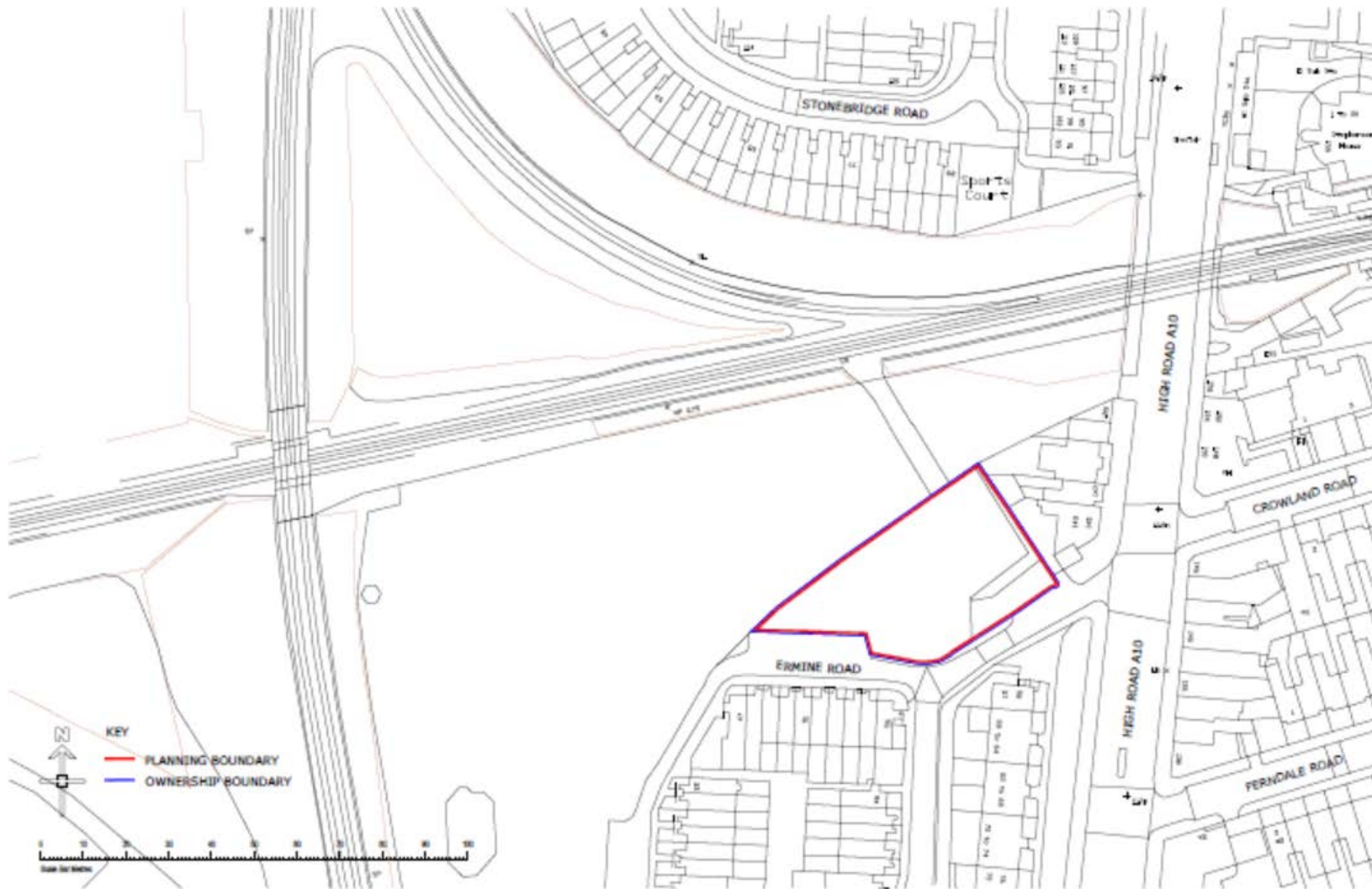
- 1) **INFORMATIVE :** In dealing with this application, Haringey Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) (Amendment No.2) Order 2012 to foster the delivery of sustainable development in a positive and proactive manner.
- 2) **INFORMATIVE : CIL**
Based on the information given on the plans, the Mayoral CIL charge will be £54,720 (912sqm x £60.55) and the Haringey CIL charge will be £19,061 (912sqm x £20.90). This would be charged in accordance with the CIL Charging Schedule and any eligible relief and includes indexation in line with the RICS CIL Index.
- 3) **INFORMATIVE : Hours of Construction Work:** The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:-
 - 8.00am - 6.00pm Monday to Friday
 - 8.00am - 1.00pm Saturday
 - and not at all on Sundays and Bank Holidays.
- 4) **INFORMATIVE : Party Wall Act:** The applicant's attention is drawn to the Party Wall Act 1996 which sets out requirements for notice to be given to relevant adjoining owners of intended works on a shared wall, on a boundary or if excavations are to be carried out near a neighbouring building.
- 5) **INFORMATIVE :** The new development will require numbering. The applicant should contact the Local Land Charges at least six weeks before the development is occupied (tel. 020 8489 5573) to arrange for the allocation of a suitable address.
- 6) **INFORMATIVE :** The London Fire Brigade strongly recommends that sprinklers are considered for new developments. Sprinkler systems installed in buildings can significantly reduce the damage caused by fire and the consequential cost to businesses and housing providers, and can reduce the risk to life. The Brigade opinion is that there are opportunities for developers and building owners to install sprinkler systems in order to save money, save property and protect the lives of occupier.
- 7) **INFORMATIVE :** With regards to surface water drainage, it is the responsibility of a developer to make proper provision for drainage to ground, water course, or a suitable sewer. In respect of surface water, it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at

the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.

- 8) INFORMATIVE : Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.
- 9) INFORMATIVE : A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed online via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.
- 10) INFORMATIVE : The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures (<https://developers.thameswater.co.uk/Developing-a-large-site/Planning-yourdevelopment/Working-near-or-diverting-our-pipes>). Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB
- 11) INFORMATIVE : The applicant should be aware that the application site is identified as an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Directions. In the event of a decision to progress the Crossrail 2 project the land may be subject to compulsory purchase in order to provide a worksite for the future delivery of the Crossrail 2 scheme.
- 12) INFORMATIVE : The applicant must seek the continual advice of the Metropolitan Police Service Designing Out Crime Officers (DOCOs) to achieve accreditation. The services of MPS DOCOs are available Free of Charge and can be contacted via docomailbox.ne@met.police.uk or 0208 217 3813.

Appendix 2 Plans and Images

Location Plan



[illegible]

Indicative room layout



Block A Elevation



① **Block A Front Elvation / Ermine Rd Section**
1 : 100



② **Block A Right Elevation**
1 : 100



③ **Block A Rear Elevation**
1 : 100



④ **Block A Left Elevation**
1 : 100

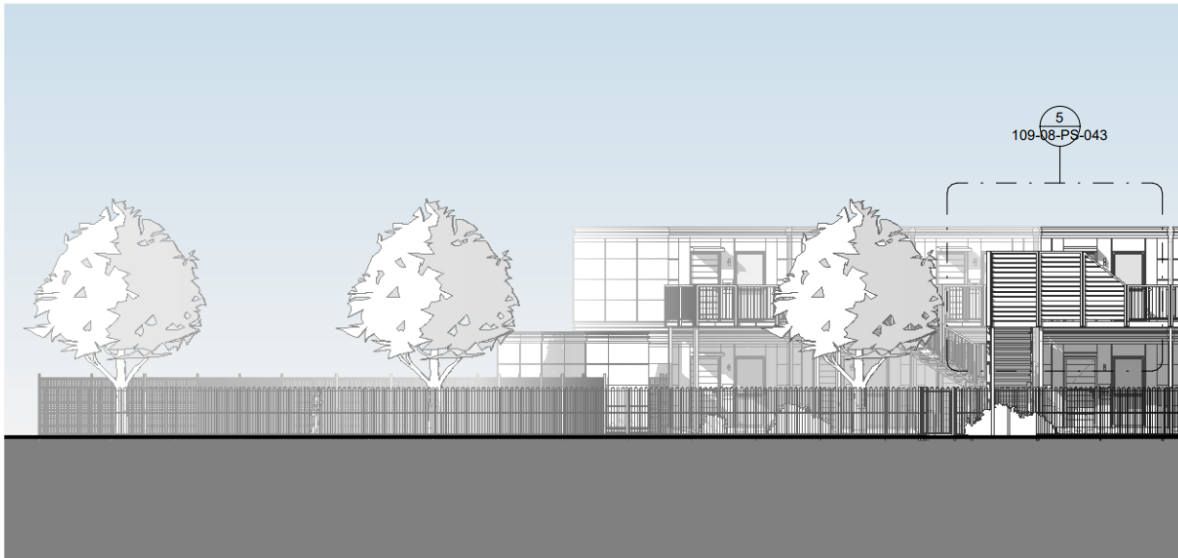


Block A Louver screening detail



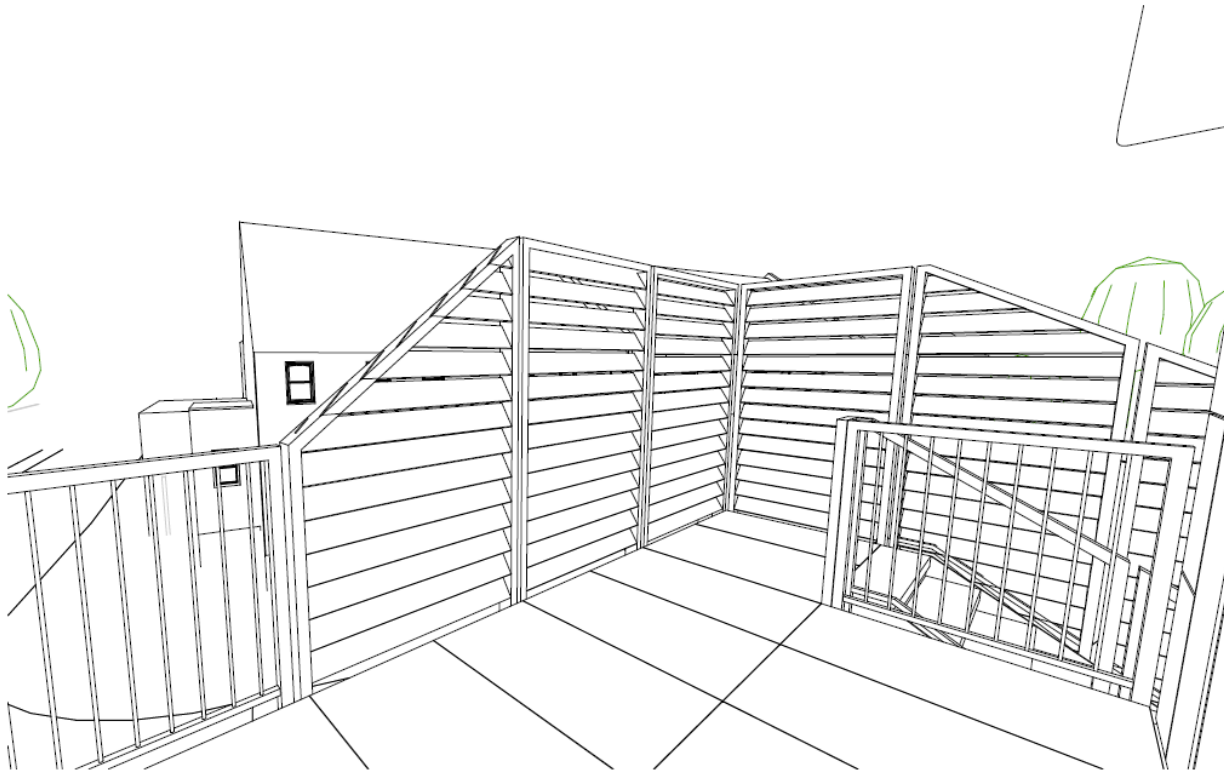
Louver Panel:
Frame to be steel to match walkway handrail
Louvers to be aluminium powder coated

5 Louver Panel Detail
1 : 50



1 Block A Front Elvation / Ermine Rd Section
1 : 100

View looking southwest from behind screening on platform access to upper storey of Block A

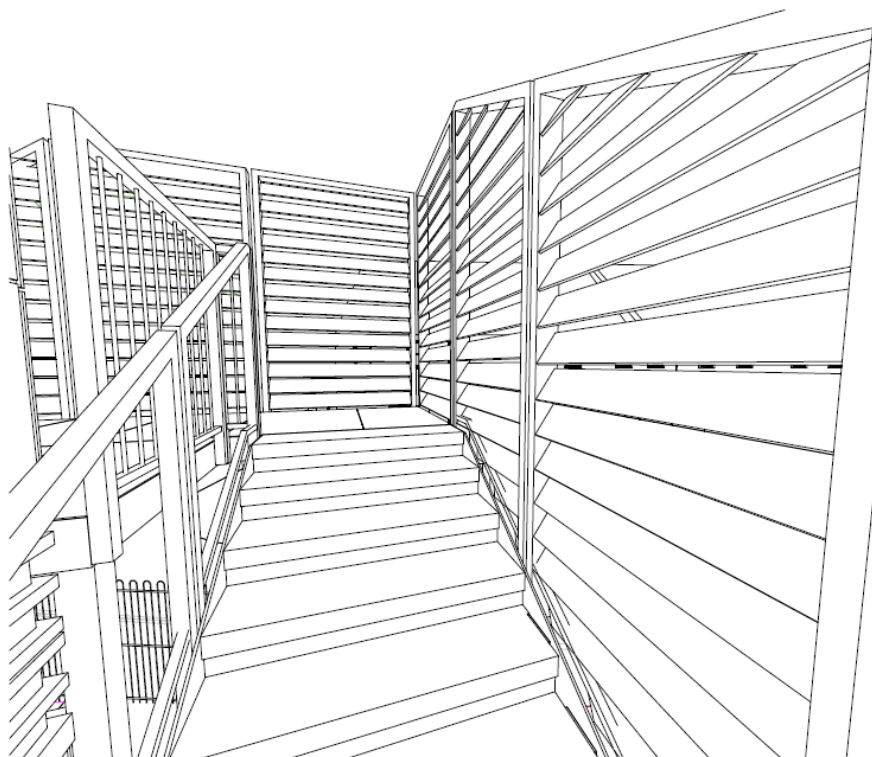


1 View 2



2 Site Layout - View 2

View looking south from behind screening on western stair access to upper storey of Block A



1 View 4



2 Site Layout - View 4

Render of view from Crowland Road / High Road – (Louvers not shown)



View from Ermine Road looking East (Louvers not shown)



View from Ermine Road looking North (Louvers not shown)



View from High Road looking northwest (Louvers not shown)



Appendix 3 Addendum to Report for Committee 11 January 2021

Planning Sub Committee 11 January 2021

ADDENDUM REPORT FOR ITEMS

UPDATE FOR CONSIDERATION AT PLANNING SUB-COMMITTEE Item No. 8

Reference No: HGY/2020/2794	Ward: Seven Sisters
Address: Land to the North of Ermine Road N15	
Proposal – Temporary planning permission for a period of 7 years to provide 38 modular units for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.	
Applicant: Hill Residential	
Ownership: Council	

1. Description Correction

The description of development has been amended so that it reads as follows:

“Temporary planning permission for a period of 7 years to provide 38 modular ~~residential homes~~ **units** for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.”

- There are 38 units but only 37 would be occupied as homes. 1 of the units would be used as a site office for staff.

2. Objections

- 6 additional objections have been received since the publication of the Officers report.
- The objections all raise concerns about a potential increase in antisocial behaviour because of the new residents and 1 objection also raised concerns about the appearance and size of the units and their siting.
- All these points have been raised previously and have been addressed in the report.

3. Fire Safety

- *The London Fire Brigade (LFB)* have responded to the additional plans provided to confirm that they are now satisfied with the proposals. (Date 7th January 2021 and attached in Appendix 1 below)

4. Environment Agency – Culvert Survey

- With reference to paras 6.10.6- 6.10.8
- The EA have recommended carrying out a ground penetrating radar (GPR) survey to see if the culvert can be located on the site itself or within 4m outside of the site along the northern boundary. The applicant has commissioned this survey and it will take place this week.
- If the survey method locates the culvert its depth would need to be established through a dig. Loading calculations would then need to be provided to show that the proposed development would have an undue impact the stability of the culvert.
- If the survey method fails to locate the culvert under the site, or within 4m of the northern boundary the EA would remove their objection. The EA have also recommended Condition 18 which reads as follows:
- This Condition is amended (additional wording in bold) as follows to ensure the survey is carried out and the advice above is followed where necessary:

Culvert protection

- 18) Prior to any above ground works, a ground penetrating radar (GPR) survey of the site and within 4m of the northern boundary shall be carried out to ascertain if the Stonebridge Brook (culvert) is located under or near to the site.

If the GPR survey finds the culvert under, or within 4m of, the site then no further development shall be carried out until a condition survey has been completed and a strategy put in place to protect the culvert. The strategy shall be submitted to and approved in writing by, the local planning authority (in consultation with the Environment Agency), and shall be implemented as approved. The strategy shall include the following components;

- Details of the location, depth, and structural condition of the culvert.
- Plans to repair any damage the culvert to ensure structural stability for the lifetime of the development.
- Details of how work would be carried out on site without damaging the structure, or impeding the function of, the culvert (including loading calculations).

If the GPR survey does not locate the culvert, then the results shall be shared with the EA and works can proceed in accordance with the other conditions of the permission.

Reason: To reduce the risk of flooding to the proposed development and future occupants, ensure the structural integrity of the existing flood defences and reduce the risk of flooding from blockages to any existing culvert in accordance with Policy DM28: Protecting and Enhancing Watercourses and Flood Defences.

Appendix 1



Fire Safety Regulation, North West 4 Team
169 Union Street London SE1 0LL
T 020 8555 1200 x89171

Minicom 020 7960 3629
london-fire.gov.uk

Private & Confidential
Philip Elliott
Principal Planning Officer
Haringey Council
Planning Service, Level 6,
River Park House,
225 High Road,
Wood Green,
N22 8HQ

London Fire and Emergency Planning
Authority runs the London Fire Brigade

Date 7th January 2021
Our Ref
Your Ref HCY/2020/2794

Dear Sir/Madam

RECORD OF CONSULTATION/ADVICE GIVEN

TOWN AND COUNTRY PLANNING ACT

SCOPE OF WORKS: 3 Blocks Of 2-Storey Modular Homes To Provide Accommodation For
People Who Have Been Homeless
PREMISES Land to the North of Ermine Rd
PLAN NUMBER(S) (if any): ERH-109-08-PS-014 & 015

The London Fire Commissioner (the Commissioner) is the fire and rescue authority for London. The Commissioner is responsible for enforcing the Regulatory Reform (Fire Safety) Order 2005 (The Order) in London.

The Commissioner has been consulted with regard to the above-mentioned premises and makes the following observations:

The Commissioner is satisfied with the proposals with the fire fighting access only

This Authority strongly recommends that sprinklers are considered for new developments and major alterations to existing premises, particularly where the proposals relate to schools and care homes. Sprinkler systems installed in buildings can significantly reduce the damage caused by fire and the consequential cost to businesses and housing providers, and can reduce the risk to life. The Brigade opinion is that there are opportunities for developers and building owners to install sprinkler systems in order to save money, save property and protect the lives of occupier. Please note that it is our policy to regularly advise our elected Members about how many cases there have been where we have recommended sprinklers and what the outcomes of those recommendations were. These quarterly reports to our Members are public documents which are available on our website.

Any queries regarding this letter should be addressed to FSR-AdminSupport@london-fire.gov.uk. If you are dissatisfied in any way with the response given, please ask to speak to the Team Leader quoting our reference.

Yours faithfully,

FS_D_01 (Rev 23, 10/03/2016)

Page 1 of 2

A handwritten signature in black ink, appearing to read "M. Howlin", is written over a horizontal line.

Assistant Commissioner (Fire Safety Regulation)

Reply to M Howlin
Direct T 0208 555 1200 Ext. 89170

Appendix 4 Committee Report for 11 January 2021

Within separate document.

Planning Sub Committee

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS

Reference No: HGY/2020/2794

Ward: Seven Sisters

Address: Land to the North of Ermine Road N15

Proposal: Temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as accommodation for people who have been street homeless, with associated cycle and refuse storage.

Applicant: Hill Residential

Ownership: Council

Case Officer Contact: Philip Elliott

Site Visit Date:

Date received: 06/11/2020 **Last amended date:** 21/12/2020

1.1 This application has been reported to Planning Sub Committee as it is a major application on Council owned land, and significant material planning objections have been received during the consultation process.

1.2 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- There is a significant need for homeless accommodation in Haringey, and this proposal provides 37 units of temporary accommodation specifically designed to meet the needs of people who have experienced homelessness. It would improve the life expectancy of residents and reduce the financial costs of temporary accommodation to the Council.
- The proposal would provide much needed temporary accommodation for 37 Haringey residents that have experienced homelessness;
- The modular construction enables the proposed homes to be built quickly so that interventions can be made as soon as possible to help those in need;
- The proposal would make a positive contribution to Ermine Road, improving the character of this vacant site.
- The proposed development would preserve the setting of the nearby Seven Sisters/Page Green Conservation Area and would have an acceptable appearance from within the Conservation Area and the locally listed Dutch House.

- There would be no material impact on parking in the area. Impacts on nearby properties would be suitably mitigated.

2. RECOMMENDATION

- 2.1 That the Committee resolve to GRANT planning permission and that the Head of Development Management is authorised to issue the planning permission and impose conditions and informatives

Conditions (the full text of recommended conditions is contained in Appendix 1 of this report)

- 1) Temporary Permission
- 2) Approved Plans
- 3) Materials as indicated on approved plans
- 4) No Parking Permits for New Residents
- 5) All Parking Delivered as Approved
- 6) Construction Management/Logistics Plan
- 7) Energy Strategy
- 8) Carbon Offset Contribution
- 9) Land Contamination
- 10) Unexpected Contamination
- 11) CEMP
- 12) Drainage / Flooding (FRA)
- 13) Provision & Retention of SuDS
- 14) No Light Spill into SINC (Bats)
- 15) Tree protection (SINC/Bats)
- 16) Waste storage delivered and made accessible
- 17) Secured by Design
- 18) Culvert Protection

Informatives

- 1) Co-operation
- 2) CIL liable
- 3) Hours of construction
- 4) Party Wall Act
- 5) Street Numbering
- 6) LFB
- 7) Thames Water Surface drainage
- 8) Thames Water Pressure
- 9) Thames Water Groundwater Risk Management Permit
- 10) Thames Water Within 15m
- 11) Crossrail 2 Safeguarding
- 12) Designing Out Crime

CONTENTS

- 3) PROPOSED DEVELOPMENT AND SITE LOCATION DETAILS
- 4) CONSULATION RESPONSE
- 5) LOCAL REPRESENTATIONS
- 6) MATERIAL PLANNING CONSIDERATIONS
- 7) RECOMMENDATION
- 8) PLANNING CONDITIONS & INFORMATIVES

APPENDICES:

Appendix 1	Planning Conditions and Informatives
Appendix 2	Plans and Images
Appendix 3	Consultation Responses – Internal and External Consultees
Appendix 4	Representations

3.0 PROPOSED DEVELOPMENT AND LOCATION DETAILS

3.1 Proposed development

- 3.1.1. Planning permission is sought for three (3) two-storey modular buildings (Blocks A, B, & C) to provide 37 supported homes for people who have been street homeless and require a period of stabilisation and support; and one (1) ancillary office for a member of staff. There would be 9 homes in Block A and 1 office, 16 homes in Block B, and 12 homes in Block C.
- 3.1.2. The proposal would be delivered by Hill Residential Limited, a large housebuilder that builds around 2,000 homes a year, including more than 1,000 for clients and partners in the affordable housing sector. The proposal is part of a programme by the Hill Group Foundation to provide 200 modular homes for people who have been street homeless over the next five years, targeting smaller sites with easy access to a city centre where homes can be managed and monitored easily by their selected charity partners.
- 3.1.3. The proposal has been developed between the Council, a wide range of statutory and voluntary partners in the borough, and The Hill Group Foundation. The development would offer strengths-based support and trauma-informed care to enable people to move on with their lives after a period of homelessness.
- 3.1.4. The proposal seeks a temporary permission for 7 years until such time as there is greater clarity on the future of the site which is safeguarded for Crossrail 2.

3.2. Site and Surroundings

- 3.2.1. The site is a parcel of land currently laid to hardstanding to the north side of Ermine Road and to the west of Tottenham High Road. The site sits within the Seven Sisters neighbourhood of Tottenham and is at the southern end of the Tottenham Area Action Plan area, set for long-term regeneration with thousands of new homes, better access to jobs and employment, and improved transport links.
- 3.2.2. To the south of the site is Ermine Road, characterised by two to three storey residential post war terraced houses. To the north east of the site on the east side of the High Road (approx. 35m) is the southern boundary of the Seven Sisters/Page Green Conservation Area. The nearest locally listed buildings are within this conservation area and include the locally listed Dutch House (on the corner of High Road / Crowland Road).
- 3.2.3. The site is bound to the north and west by green open space, designated as an Ecological Corridor (Seven Sisters east/west link) & a Site of Importance for Nature Conservation (SINC) Grade II [Tottenham Railsides] that surrounds nearby

railway lines. The site is designated as an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Direction to provide a worksite for the future delivery of the Crossrail 2 scheme.

- 3.2.4. To the east is a block with commercial on the ground floor and two storeys of residential above, beyond this is the High Road (A10) a principle A-road, providing north and south routes towards Central London and the North Circular (A406). The site sits between these commercial properties facing Tottenham High Road to the east, the Ermine Road estate (1970s council housing) to the south, and the Ermine Triangle green space to the north-west.
- 3.2.5. Ermine Road contains Cycle Superhighway 1 and although the road can be accessed by vehicles from the High Road, bollards prevent vehicles travelling beyond the western elevation of the building to the south of Ermine Road (which faces onto High Road to the east).
- 3.2.6. The site and the whole surrounding area falls within a critical drainage area (CDA). Historically the Stonebridge Brook could be found to the north/northeast of the site. This main river was historically culverted and is located at least 8m away from the northeast (rear) boundary of the site.
- 3.2.7. It is understood that the site is an area of leftover land originally intended, when the railways were built for a connecting rail loop between two crossing railways (both now parts of the London Overground passenger network); the Liverpool Street to Enfield Town line and the Gospel Oak to Barking line but this was never built and is no longer necessary

3.3 Relevant Planning and Enforcement history

- There is no relevant planning or enforcement history.

4. CONSULTATION RESPONSE

4.1. Application Consultation

4.1.1. The following were consulted regarding the application:

- The following responses were received (Below is a summary of the received comments. Some responses have been received that are more comprehensive than what is shown below – these responses are shown in more detail in Appendix 3):

Internal:

1) Design

- *It provides an exemplary example of modular building design, in a form that is of gentle density and appearance – which is compatible with the varied but mostly residential context.*
- *There would be a secure, safe, and clearly defined boundary and definition of public, private communal, and private space, with active street frontage.*
- *Green landscaping proposed for the site will soften its appearance and be compatible with the neighbouring site of nature importance.*

2) Conservation

- *The proposed development would not have a negative impact on the significance of the conservation area and associated heritage assets, including the locally listed Dutch House.*

3) Transportation

- *The transport demands of this specialist form of housing is expected to be different from conventional housing. Flexibility could be exercised in provision for car parking and for servicing and deliveries.*
- *Conditions requiring a Construction Management/Logistics Plan (CMP/CLP); and a Delivery and Servicing Management Plan are recommended; as well as a s106 / s278 agreement for any highway works – to:*
 - *restrict occupiers from obtaining CPZ parking permits,*
 - *identify where the highway may be affected by proposals,*
 - *relocate / remove existing bollards,*
 - *allow for highway improvements / reinstatement of kerb over redundant crossovers, and*
 - *provide a public footway along the site frontage, in consultation with TfL.*

4) Carbon Management (Energy, offset, overheating, sustainability)

- *No objection subject to the development being constructed in strict accordance with the Carbon Reduction Statement, Technical Information Report, and Overheating Study; and subject to a carbon offset contribution of £7,454.65 being paid.*

5) Carbon Management (Pollution – Contamination & Air Quality)

- *No objection to the development in relation to Air Quality and Land Contamination but conditions relating to Land Contamination, Unexpected Contamination, & a Construction Environmental Management Plan (CEMP) are recommended should planning permission be granted.*

6) Nature Conservation / Ecology

- *The report and mitigating measures proposed are acceptable and should be conditioned into the development.*

7) Waste

- *This proposal will need 5 x 1100L waste receptacles for refuse and 3 x 1100L waste receptacles for dry recycling. Waste containers must be located no further than 10 metres from the point of collection.*

8) Lead Local Flood Authority (LLFA) - Drainage / Flooding (SuDS)

- *The LLFA are happy for the proposal to proceed at this stage subject to additional information about water network connection approval and delivery of SuDS solutions that are managed and maintained effectively for the lifetime for the development.*

9) Noise ASB

- *No response to date.*

10) Building Control

- *No response to date.*

External:

11) Environment Agency (EA)

- *The EA have reason to believe the site is within 3 metres of a culverted main river, the Stonebridge Brook, and have raised an objection given this close proximity. [Officer Note: a survey has been conducted which has not located the culvert. Further information will be provided in an addendum report. A condition is also recommended (no.18)].*

12) London Fire Brigade (LFB)

- *The Commissioner is not satisfied with the proposals for fire fighting access as the plans do not confirm if it would be possible to get to within 45 meters of fire hose length to the farthest room on the farthest block. [Officer Note: a further plan has been produced to seek to satisfy this concern. Further information will be provided in an addendum report].*

13) Designing Out Crime Officer - Metropolitan Police Service

- *The drawings take our concerns into consideration and provide a safer environment for the residents of the development to enjoy as well as the wider area. I cannot see any reason why the development cannot achieve accreditation according to SBD guidelines.*

14) Transport for London (TfL)

- *TfL have no objections to the proposal; they require further information on cycle parking and require a Construction Management/Logistics Plan (CMP/CLP) to be submitted.*

15) Thames Water

- *No objection regarding waste water network and sewage treatment works infrastructure capacity, based on the information provided; & No objection with regard to water network and water treatment infrastructure capacity.*

16) GLAAS

- *The proposal is unlikely to have a significant effect on heritage assets of archaeological interest. No further assessment or conditions are therefore necessary.*

17) Natural England

- *Natural England has no comment on this application with regards to designated sites.*

18) Network Rail

- *Network Rail have no objections to the proposals.*

19) London Overground

- *London Overground Infrastructure had no comments on the proposals.*

20) Crossrail 2 (Safeguarding Direction)

- *The site is an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Directions. In the event of a decision to progress the Crossrail 2 project the land may be subject to compulsory purchase to provide a worksite for the future delivery of the Crossrail 2 scheme.*

5. LOCAL REPRESENTATIONS

5.1 The following were consulted:

- 206 Neighbouring properties
- Ermine Road Residents Association
- 3 site notices were erected close to the site

5.2 The number of representations received from neighbours, local groups etc in response to notification and publicity of the application were as follows:

- Number of individual responses:

- Objecting: 59
- Supporting: 1 (Summarised as - *Homeless people need somewhere to live*)
- Others: A petition against the proposal was received which indicates that residents from 54 local households wish to object to the proposals. It is understood that some of the signatories wish to remain anonymous, so their names and addresses were not displayed on the public register. Anonymous

objections are generally considered to have less weight than those where names and addresses are provided.

5.3 The following local groups/societies made representations:

- No representations received from local groups/societies

5.4 The following Councillor made representations:

- Councillor Barbara Blake:
 - Residents who live near this site are concerned that there could be an increase in anti-social behaviour.
 - There appears to be no specific plan in place to ensure this does not happen other than the existing structures which are overstretched.
 - There is little confidence that the site will be managed well in terms of general maintenance - landscaping, refuse collection, car parking but also managing the day to day needs of vulnerable people who live in these units.
 - Very vulnerable people will be housed here yet there seems to be very little in the way of facilities.
 - Residents are also concerned about the proximity of the units and that some of them will be overlooked.

5.5 The issues raised in representations that are material to the determination of the application are set out in Appendix 4 and summarised as follows:

- **Potential for an increase in crime** (antisocial behaviour)
- **Noise & disturbance**
- **Impact on neighbouring amenity & parking**
 - light pollution; overshadowing; overlooking; and privacy
 - waste management
- **Design**
 - The orange colouring would be obtrusive
- **Scale/no. of units and the size of the units**
 - Too many units/people
 - The units are too small

5.6 The following issues raised are not material planning considerations:

- Loss of a private view (This is a private matter and therefore not a material planning consideration)
- Impact on property values (This is a private matter and therefore not a material planning consideration)

6 MATERIAL PLANNING CONSIDERATIONS

6.1 The main planning issues raised by the proposed development are:

1. Principle of the development
2. Impact on the amenity of adjoining occupiers
3. Character and appearance and setting of the nearby conservation area
4. Design & Secured by Design
5. Quality of Residential Accommodation
6. Parking and highway safety
7. Energy and Climate Change
8. Pollution – Air quality & contamination
9. Flood risk & Drainage
10. The impact on the adjacent ecological asset
11. Waste & recycling

6.2 Principle of the development

Policy Background

6.2.1 The National Planning Policy Framework NPPF was updated in July 2018 and minor clarifications to the revised version were published in February 2019. The NPPF establishes the overarching principles of the planning system, including the requirement of the system to “drive and support development” through the local development plan process.

The Development Plan

6.2.2 For the purposes of S38(6) of the Planning and Compulsory Purchase Act 2004 the Local Plan comprises the Strategic Policies Development Plan Document (DPD), Development Management Policies DPD and Tottenham Area Action Plan (AAP) and the London Plan (2016).

The London Plan

6.2.3 The London Plan is the overall strategic plan for London, setting out an integrated economic, environmental, transport and social framework for the development of London over the next 20–25 years. The consolidated London Plan (2016) sets several objectives for development through various policies. The policies in the London Plan are accompanied by a suite of Supplementary Planning Guidance (SPGs) that provide further guidance.

6.2.4 In December 2019, the Mayor published an ‘Intend to Publish London Plan’. On 13 March 2020 & 10 December 2020, the Secretary of State issued Directions to change several proposed policies. In response to the latest direction the Mayor has formally approved a new London Plan, the ‘**Publication London Plan**’. The Publication London Plan has been sent to the Secretary of State for his consideration.

- 6.2.5 In line with paragraph 48 of the NPPF, the weight attached to this Plan should reflect the stage of its preparation; the extent to which there are unresolved objections to relevant policies; and the degree of consistency of the relevant policies in the emerging Plan to the NPPF.
- 6.2.6 Whilst the published London Plan (2016) remains part of Haringey's Development Plan, given the advanced stage that the Publication London Plan has reached, significant weight can be attached to it in the determination of planning applications (although there is greater uncertainty about those draft policies that are subject to the Secretary of State's Direction).

The Local Plan

- 6.2.7 The Strategic Policies DPD sets out the long-term vision of how Haringey, and the places within it, should develop by 2026 and sets out the Council's spatial strategy for achieving that vision. The Site Allocations development plan document (DPD) and Tottenham Area Action Plan (AAP) give effect to the spatial strategy by allocating sufficient sites to accommodate development needs.

Tottenham Area Action Plan (AAP)

- 6.2.8 The Tottenham AAP sets out a strategy for how growth will be managed to ensure the best quality of life for existing and future Tottenham residents, workers, and visitors. The plan sets area wide, neighbourhood and site-specific allocations.
- 6.2.9 The AAP indicates that development and regeneration within Tottenham will be targeted at four specific neighbourhood areas including Seven Sisters where this site is located.
- 6.2.10 Development proposals incorporating a housing element will be expected to provide the housing in accordance with the minimum capacities, set out in the Site Allocations in this AAP. Higher densities and capacities may be acceptable in appropriate locations, close to town centres, in areas with good local facilities and amenities and in areas well served by public transport, providing the other policies of this AAP and Haringey's Local Plan are not compromised. The application site meets the four listed criteria above.

Housing

- 6.2.11 To improve the diversity and choice of homes and to support sustainable communities in Tottenham, the Council will seek the delivery of 10,000 additional new homes across the Tottenham AAP area to meet housing needs, contribute to inclusive and mixed communities and to improve the quality of homes.

Assessment of the proposal

- 6.2.12 In Haringey, and across London, levels of homelessness and rough sleeping are unacceptably high. Emerging Policy H12 of the Publication London Plan notes that the delivery of supported and specialised housing for rough sleepers should be supported in principle.
- 6.2.13 The Council is committed to reducing homelessness and rough sleeping. There is not enough social housing to meet demand from homeless people, and the increasing reliance on the private rented sector to offer solutions is costly and often provides unsatisfactory and unsuitable housing.
- 6.2.14 The Haringey Local Plan (DMDPD) recognised that secured accommodation can provide suitable temporary accommodation subject to appropriate management and safeguards and can help integrate vulnerable people into the community.
- 6.2.15 Homelessness is the symptom of a complex interaction between a person's specific situation and a range of structural and social factors that are often beyond their control. To successfully tackle homelessness, the Council must increase the supply of accommodation available and meet the needs of those already in temporary accommodation.
- 6.2.16 This proposal would deliver an ongoing reduction in rough sleeping and address the multiple harms it brings to individuals and communities. The proposed modular construction would enable a rapid delivery to offer a route off the street/out of unsuitable and expensive accommodation for Haringey residents. It would improve health, wellbeing and resilience and tackle street activity associated with rough sleeping.
- 6.2.17 There is strong Policy support for purpose built affordable housing to meet specific needs. Strategic Policy SP2 (Housing) of Haringey's Local Plan aims to provide high quality new residential development in Haringey by maximising housing for people whose circumstances makes them vulnerable and/or people with specific needs.
- 6.2.18 The site is a brownfield site within a residential area so there is policy support for residential development on the site.
- 6.2.19 Policy H12 Supported and specialised accommodation of the Publication London Plan states that the delivery of supported and specialised housing which meets an identified need should be supported. It goes on to state that the form this takes will vary, and it should be designed to satisfy the requirements of the specific use or group it is intended for. It then goes on to list what Supported and specialised accommodation could include. The list includes:
- accommodation for people leaving hostels, refuges, and other supported housing to enable them to live independently
 - accommodation for rough sleepers.

6.2.20 Paragraph 4.12.2 of the Publication London Plan notes that Supported and specialised accommodation can include accommodation that incorporates support for a particular group, sometimes until they are ready to move on. The proposed development would seek to do this.

6.2.21 Policy DM15: Specialist Housing of the Development Management DPD (DMDPD) supports proposals for new special needs housing. It sets out the criteria for considering special needs housing, which must show that:

- a) There is an established local need for the form of special needs housing sought having regard also to the aims and recommendations of Haringey's Housing Strategy and Older People Strategy;*
- b) The standard of housing and facilities are suitable for the intended occupiers in terms of:*
 - i. The provision of appropriate amenity space, parking, and servicing;*
 - ii. The level of independence; and*
 - iii. Level of supervision, management, and care/support;*
- c) There is a good level of accessibility to public transport, shops, services, and community facilities appropriate to the needs of the intended occupiers; and*
- d) The impact of the proposed development would not be detrimental to the amenity of the local area or to local services.*

6.2.22 Each of these requirements is considered below.

a) Established Local Need

6.2.23 Haringey's Homelessness strategy (2018) notes that homelessness is a problem that affects Haringey particularly severely; with the second highest level of homelessness in the country it advocates a radical new approach to reduce the multiple harms that homelessness causes. The strategy is a material consideration.

6.2.24 It notes that rough sleeping is the most visible form of homelessness and the most damaging. It causes significant harm to individual physical and mental health, and general wellbeing and reduced life expectancy. A female sleeping on the streets has a life expectancy of 43 years and a male 47 years, the UK national average is approximately 83 years for females and 79 years for males.

6.2.25 There is a significant need within Haringey for supported housing for people who have been street homeless and require a period of stabilisation and support to regain their confidence, health, and independence. Haringey's Housing Strategy

2017-2022 (HS) notes (pg18) that it will seek to provide suitable and affordable temporary accommodation, while overall reducing its cost.

6.2.26 Like many other boroughs, Haringey Council finds it increasingly difficult to secure good quality, sustainable, and affordable temporary accommodation. Competition for private rented homes has driven up prices while the council is dealing with rising levels of homelessness meaning households spend longer in temporary accommodation.

6.2.27 The proposed development has been designed using relationships and expertise developed between the Council and a wide range of statutory and voluntary partners within Haringey. The service proposes to offer strengths-based support and trauma-informed care to enable people to move on with their lives after a period of homelessness.

6.2.28 This is in line with the approach advocated in the HS under section 6.2 (Taking new approaches to temporary accommodation) which states that in order for the Council to secure good quality, sustainable and affordable temporary accommodation they will:

- Forge new partnerships with investors offering long term investment to provide affordable, good quality, secure homes to help homeless households as well as additional, cheaper temporary accommodation.
- Maximise the supply of affordable high quality temporary accommodation inside the borough.
- Use innovative solutions, for example Modern Methods of Construction, for rapid provision of homes on sites suited to this use.

6.2.29 The proposal meets the preference of the HS (pg20) to provide temporary accommodation within Haringey. It is very difficult to secure the volume of homes needed at a cost that is affordable for the resident and will not require additional subsidy by the council. The method of construction also allows this to be delivered at the earliest opportunity.

6.2.30 The proposal would enable the Council to address the damage rough sleeping has had on the physical and mental health, and general wellbeing of Haringey residents that have been street homeless. It would help to provide coordinated shelter and support to increase life expectancies and break the cycle of suffering that the people affected can experience.

b) Standard of Housing

6.2.31 The proposal has been devised by those that deliver this type of supported housing for people who have been street homeless, as well as by former and prospective service users themselves.

- 6.2.32 It has been designed to meet the specific needs of residents through homes that provide an appropriate level of independence, supervision, management, and care/support to enable a period of stabilisation in combination with strengths-based support and trauma-informed care to enable the residents to move on with their lives after a period of homelessness.
- 6.2.33 The proposal provides housing and support in one location, within Haringey, which engenders a solution that is likely to have better outcomes for the occupants at a lower cost than alternative temporary housing solutions that are available which is in line with the Council's Housing Strategy (2018) .
- 6.2.34 The proposed homes would be 24sqm which is smaller than the 37sqm recommended for a 1-bed flat with a shower room in the Technical housing standards produced by the Department for Communities and Local Government. They have been designed as specialist housing for temporary accommodation to support people who have been street homeless at a low cost.
- 6.2.35 The homes have been designed to be large enough to enable a good quality home for people that have experienced homelessness, while ensuring residents can manage the home and are not vulnerable to exploitation through 'cuckooing' (a practice where people take over a person's home and use the property to facilitate exploitation).
- 6.2.36 As such, the shortfall is considered acceptable in this instance, for it to provide specialist housing in a short time and at a low cost - to address a significant housing need. The residential quality is assessed in more detail in the residential amenity section below. This does not set a precedent for smaller self-contained flats in the borough as weight has been given to the very-specific circumstances of this case.

c) Accessibility to public transport, shops, services, and community facilities

- 6.2.37 The site has a high level of accessibility to public transport with a PTAL 6a one of the highest possible due to proximity to South Tottenham Station, Seven Sisters Underground Station, and numerous bus routes on the High Road. It is also close to Cycle Superhighway 1 which runs along Ermine Road. The site is in a location well served by shops, services, and community facilities – being on Tottenham High Road.

d) Impact on amenity of the local area or to services

- 6.2.38 The proposal would not be detrimental to the amenity of the area as set out in detail in the following sections of the report. It is on a vacant site within a residential area which has issues with fly tipping, illegal occupation, and antisocial behaviour. The proposal is relatively modest in scale so would not impact on neighbouring residents.

- 6.2.39 The proposal would not have a detrimental impact on local services – the residents would have access to on-site strengths-based support and trauma-informed care during the day and a concierge at night; residents would largely be supported within the site.

Crossrail 2 Safeguarding

- 6.2.40 The London Plan (2016) Policy 6.4 states that the Mayor will work with strategic partners to improve the public transport system in London and increase public transport capacity by developing Crossrail 2. This commitment is carried into the Publication London Plan Policy T3 which states that in development decisions, particular priority should be given to securing and supporting the delivery of Crossrail 2. The site is designated as an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Direction to provide a worksite for construction. The proposal is for a period year permission for the buildings. Due to their modular construction they are demountable and can be moved to another site in future if the site is required for the delivery of Crossrail 2.
- 6.2.41 TFL raise no objections. The proposal would therefore not hinder the delivery of Crossrail 2 and makes use of the site for housing in the interim before Crossrail is delivered.

6.3 Impact on the amenity of adjoining occupiers

- 6.3.1 The London Plan (2016) Policy 7.6 Architecture states that development must not cause unacceptable harm to the amenity of surrounding land and buildings. Publication London Plan Policy D3 Optimising site capacity through the designed approach states that the experience of development proposals should deliver appropriate outlook, privacy, and amenity; and achieve safe, secure and inclusive environments.
- 6.3.2 DMDPD Policy (2017) DM1 'Delivering High Quality Design' states that development proposals must ensure a high standard of privacy and amenity for the development's users and neighbours.

Fear of crime

- 6.3.3 Several concerns have been raised by local residents in relation to a potential increase in crime from the new residents. All occupants would be referred to the service using a robust referral process, led by the Haringey Street Outreach Team and the Council's Homelessness Pathway Team.
- 6.3.4 There would be no direct access to the service without a prior referral and assessment of suitability. This ensures that vulnerabilities and support needs are

explored with each person before they move in. This approach ensures the safety of the occupants as well as that of others.

- 6.3.5 The site design is intended to reduce opportunities for anti-social behaviour by creating a well-lit and visible environment with appropriate but not invasive CCTV. There would always be a member of staff on site. This would be a support worker in the daytime and a concierge at night. The staff office is positioned with good visibility across the site which would enable the management of visitors to the site at all times.
- 6.3.6 The scheme has also been reviewed by a Designing out Crime Officer at the Metropolitan Police who considers the development can achieve accreditation according to Secured by Design (SBD) guidelines. A number of the SBD Officer's recommendations have been discussed with the applicant. These include specific boundary / fencing requirements; removal of non-transparent panels on external stair landings (for natural surveillance); enclosure of space under stairwells (to prevent loitering); CCTV; lighting; and a postal strategy (to discourage unsolicited visits and postal theft).

Noise & disturbance

- 6.3.7 Objections have also been made relating to concerns that there would be an increase in noise and disturbance from the new occupants. As with the concerns relating to a potential increase in crime, there would always be a member of staff present who could deal with any issues in this regard.

Light spill

- 6.3.8 Concerns have been raised about light spillage and light pollution from the development. The site is opposite a residential area that is near to Tottenham High Road. There are several street lamps in the vicinity of the site and although there would be lighting in the scheme, this would be commensurate with what is in the locality and, as such, would not result in a material change that would result in harm to the amenity of nearby residents.

Impact on views from neighbouring buildings

- 6.3.9 Given the low height of the buildings and their siting and orientation away from neighbouring properties, there would be no overshadowing of adjacent properties. The closest block to properties on Ermine Road (Block A) would be largely face the highway and would appear as an obtrusive feature when viewed from nearby neighbouring properties.
- 6.3.10 The low height and siting of the buildings within the site would mean that they would not have an overbearing presence on nearby neighbouring properties and would not result in an undue sense of enclosure.

Privacy

6.3.11 The orientation of the buildings would limit overlooking from the site. Block C is orientated so that it faces west, and Block B is located at the back of the site behind Block A. Block A would be the closest block to the existing properties to the south of Ermine Road. However, the distance between these properties would be commensurate with the character of the locality.

6.3.12 Block A would comprise a total of 10 units with 5 at ground floor and 5 above. 1 of the units would be the site office. Block A would be set back from the site boundary and would be orientated towards the southeast – facing the T-junction in this part of Ermine Road.

6.3.13 Block A would not directly face the properties to the south of Ermine Road and given the distances between them (approx. 12m), the level of overlooking would be commensurate with the character of the locality. The proposed units also have relatively modest windows and given the siting and orientation; the privacy of existing residents would not be adversely affected to a material degree by the proposal.

6.4 Character and appearance and setting of the nearby conservation area

6.4.1 London Plan Policy 7.8 and Policy HC1 of the Publication London Plan requires development affecting heritage assets and their settings to conserve their significance by being sympathetic to their form, scale, and architectural detail.

6.4.2 Policy SP12 ‘Conservation’ states that the Council shall ensure the conservation of the historic significance of Haringey’s heritage assets, their setting, and the wider historic environment. The policy states that the Historic Environment should be used as the basis for heritage-led regeneration and as the basis for good design and positive change and, where possible, development should help increase accessibility to the historic environment.

6.4.3 DMDPD Policy DM9 ‘Management of the Historic Environment’ states that development which conserves or enhances the significance of a heritage asset and its setting will be supported. Proposals affecting a designated or non-designated heritage asset and its setting will be assessed against the significance of the asset and its setting, and the impact of the proposals on that significance.

6.4.4 The proposed development falls within the setting of the Seven Sisters/Page Green Conservation Area. A rendered image has been submitted which shows the proposed development from the corner of Crowland Road and the High Road, opposite (south) of the locally listed Dutch House and just outside the Conservation Area.

6.4.5 The proposed development would only be partially visible from the southernmost part of the Conservation Area, as it would be mostly screened by the terrace on

the western side of the High Road. Due to its modest mass and scale, it would be consistent with the surrounding townscape and would not detract from it.

- 6.4.6 Therefore, the proposed development would not result in material harm to the significance of the Conservation Area and associated heritage assets, including the locally listed Dutch House.
- 6.4.7 The Conservation Officer has noted that it would be desirable for the materials and colour palette of the development to reflect materials used in the local area, rather than contrast with them. However, this method of construction would not allow brick or a brick cladding panel to be used. Furthermore, the scale of the building would be modest and its siting (with the larger two blocks set back in the site) would mean that the building would have an acceptable appearance.

6.5 Design

- 6.5.1 DMDPD Policy (2015) DM1 'Delivering High Quality Design' states that development proposals should relate positively to their locality, having regard to, building heights, form, scale & massing prevailing around the site, urban grain, sense of enclosure and, where appropriate, following existing building lines, rhythm of any neighbouring or local regular plot and building widths, active, lively frontages to the public realm, and distinctive local architectural styles, detailing and materials.
- 6.5.2 Local Plan (2017) Policy SP11 states that all new development should enhance and enrich Haringey's built environment and create places and buildings that are high quality, attractive, sustainable, safe, and easy to use. Development shall be of the highest standard of design that respects its local context and character and historic significance, to contribute to the creation and enhancement of Haringey's sense of place and identity which is supported by London Plan (2016) Policies 7.4 and 7.6 and Publication Policy D3.
- 6.5.3 The Council's design officer has been consulted and notes that the development provides an exemplary example of modular building design, in a form that is of gentle density and appearance – which is compatible with the varied but mostly residential context. The 2 storey scale of development would be within and below the prevailing height and bulk of the surroundings.
- 6.5.4 The rhythm of windows and doors, panels, and colours, would provide an attractive elevational / fenestration pattern of a comparable scale and proportion to the ordinary domestic surroundings, whilst being clearly of contemporary appearance expressing its progressive modern construction.
- 6.5.5 In terms of the layout, they note that the development would make a significant contribution to improving animation and liveliness to the street. There would be a secure, safe, and clearly defined boundary and definition of public, private

communal, and private space, with active street frontage. Green landscaping proposed for the site will soften its appearance and be compatible with the neighbouring site of nature importance.

- 6.5.6 The rest of the site layout and enclosures would provide clear, secure boundaries, avoiding creating unobserved ambiguous corners and providing a clear pedestrian route from street to site entrance and site entrance to flat entrance, with stairs to access balconies to 1st floor flats all opening off and clearly visible from the central space.
- 6.5.7 In terms of residential quality, they note that the central space will provide a convivial communal meeting and amenity space, benefiting from grassed and paved areas to provide a variety of external amenity in addition to the private external amenity outside each flat's front door.
- 6.5.8 Overall, the proposal is considered a high quality design that is appropriate for the site.

Secured by Design

- 6.5.9 Publication London Plan Policy D11 Safety, security and resilience to emergency states that Boroughs should work with their local Metropolitan Police Service 'Design Out Crime' officers to identify the community safety needs, policies and sites required for their area to support provision of necessary infrastructure to maintain a safe and secure environment and reduce the fear of crime.
- 6.5.10 Local Plan Policy Sp11 'Design' states that all new development should incorporate solutions to reduce crime and the fear of crime, such as promoting social inclusion; creating well-connected and high quality public realm that is easy and safe to use; and by applying the principles set out in 'Secured by Design'.
- 6.5.11 The Designing Out Crime Office of the Metropolitan Police has been consulted on the application and involved in the design process. They have no objections to the proposals on the site subject to conditions and an informative offering recommendations to minimise crime and the fear of crime within the development and the local community.
- 6.5.12 A Condition is recommended that a 'Secured by Design' accreditation shall be obtained for the development prior to occupation and the features that are agreed to be necessary for security are retained for the lifetime of the development. This would ensure that the development is safe and secure and would reduce crime and the fear of crime.

6.6 Quality of Residential Accommodation

- 6.6.1 London Plan (2016) policy 3.5 and Publication Policy D6 requires the design of all new housing developments to enhance the quality of local places and for the dwellings to be of sufficient size and quality. Local Plan (2017) Strategic Policy SP2 and Policy DM12 of the Development Management DPD 2017 reinforce this approach. The Mayor's Housing SPG sets out the space standards for new residential developments to ensure an acceptable level of living accommodation is offered.
- 6.6.2 The proposed homes would be smaller than the nationally described space standard for a 1-bed, 1-person self-contained home which has a shower room (37sqm). 37sqm is also the requirement within the London Plan (2016) and the Publication London Plan (2020). The proposed homes would be 24sqm.
- 6.6.3 The proposed housing has been specifically designed to enable people who have been street homeless to stabilise their situation for a temporary period whilst also being able to get access to support to regain their confidence, health, and independence. It is intended that the homes do not provide any additional or excess space over what is required to meet their needs.
- 6.6.4 This type of housing and the support services being offered are not being delivered within more traditional forms of housing given the high development costs. Essentially, the modular build type maximises the number of homes it can provide whilst still offering a good level of accommodation that would otherwise be delivered in a more spread out, unsuitable, and costly fashion through temporary accommodation rented from the private sector.
- 6.6.5 The modular building design would also enable the rapid provision of homes to enable support and accommodation to be provided for people who have experienced rough sleeping at the earliest opportunity. This could not be viably delivered in any other format given the capital expenditure involved.
- 6.6.6 As such, allowances should be made on the space standards as the public benefits in this regard outweigh any perceived harm.

6.7 Parking and highway safety

- 6.7.1 Local Plan (2017) Policy SP7 Transport states that the Council aims to tackle climate change, improve local place shaping and public realm, and environmental and transport quality and safety by promoting public transport, walking and cycling and seeking to locate major trip generating developments in locations with good access to public transport. This is supported by DM Policy (2017) DM31 'Sustainable Transport'.
- 6.7.2 DM Policy (2017) DM32 'Parking' states that the Council will support proposals for new development with limited or no on-site parking where there are alternative

and accessible means of transport available, public transport accessibility is at least 4 as defined in the Public Transport Accessibility Index (PTAL), a Controlled Parking Zone (CPZ) exists or will be provided prior to the occupation of the development, parking is provided for disabled people; and parking is designated for occupiers of developments specified as 'car capped' which means that no parking other than the space on site is available to residents or staff.

- 6.7.3 The site is close to alternative and accessible means of transport, it has a high PTAL, and a Controlled Parking Zone (CPZ) exists near to the site. One parking space would be provided for disabled people in line with Publication London Plan standards. It is therefore acceptable for the scheme to be car capped.
- 6.7.4 Ordinarily a legal agreement would also be entered into which would restrict occupiers from obtaining CPZ parking permits. However, the temporary and specialist nature of the housing must be considered when assessing whether this is required by the development. These are homes would be steppingstones to help people transition from rough sleeping. The proposal would give residents time to adjust to living in their own place and to develop their independent living skills. Following this they will move on to their own permanent homes.
- 6.7.5 As such, it is highly unlikely given the above and the length of tenancy that residents would own a car. In any event, a condition is recommended which would ensure that the development would not be occupied other than through a lease/license that ensures that residents are obligated not to apply for a parking permit for any CPZ.

Deliveries & Servicing

- 6.7.6 This is a specialist form of supported housing that has been specifically designed to enable people who have been street homeless to stabilise their situation for a temporary period. It is intended that the homes do not provide any additional or excess space over what is required to meet their needs. Given the specialist nature of the housing, it is unlikely that the development would incur a similar level of delivery/servicing trips as a typical residential development with 37 homes.
- 6.7.7 In any event, the development has provided a parking space on site only for deliveries and emergency vehicles and a condition is recommended to ensure it is only used as such. Given the nature of the proposed use this level of provision would be appropriate to deal with the likely volume of deliveries and servicing. As such, a management plan to address these issues would be unnecessary.

Cycle Parking

- 6.7.8 The proposal would provide 38 cycle parking spaces that would all be secure, sheltered, & accessible. The total of 38 includes 36 Spaces for long stay which includes 4 spaces for larger cycles and 2 short stay spaces. This would result in a deficit of 1 space from the minimum required under Policy T5 of the Publication London Plan.

- 6.7.9 Another space could be accommodated within the site to meet the standard, but it would disrupt the layout and pedestrian movements within the site, and it would not be sheltered. Therefore, it would be less secure and, as a result, much less likely to be used. It is also highly unlikely that all residents would always require a cycle parking space. Given these circumstances, the provision of an additional space to meet the standard would be unnecessary.

Construction Management/Logistics

- 6.7.10 Whilst the period of construction for modular developments is generally rapid compared to typical forms of development, there will still be disruption from the traffic movements associated with the delivery and craning onto the site of the buildings. A condition has been recommended (as advised by the Council's Transportation Officers and TfL) which would require a Construction Management/Logistics Plan (CMP/CLP) to be submitted for approval prior to these works being carried out to ensure that disruption is minimised.
- 6.7.11 The proposed development does not propose any highway works. The existing crossovers and footways are maintained. In the absence of any works being made to the highway, a s278 agreement would not be required.
- 6.7.12 A request has also been made for the development to deliver highway improvements and provide a public footway along the site frontage, so that the northern side of Ermine Road has a continuous footway connecting to the High Road. Given the temporary nature of the development the improvements could be superseded after the 7 year period, so it is unreasonable and unnecessary for the development to provide this.

6.8 Energy and Climate Change

- 6.8.1 The NPPF and London Plan Policies 5.1, 5.2, 5.3, 5.7, 5.8, 5.9, 5.10 and 5.11, and Local Plan Policy SP4 sets out the approach to climate change and requires developments to meet the highest standards of sustainable design, including the conservation of energy and water; ensuring designs make the most of natural systems and the conserving and enhancing the natural environment.
- 6.8.2 The London Plan requires all new homes to achieve a zero carbon target beyond Part L 2013 of the Building Regulations. The Publication London Plan further confirms this in Policy SI2. The London Plan also sets a target of 25% of the heat and power used in London to be generated using localised decentralised energy systems by 2025. Where an identified future decentralised energy network exists near a site it will be expected that the site is designed so that it can easily be connected to the future network when it is delivered.

- 6.8.3 The applicant is not proposing any Be Clean measures. The site is not within reasonable distance of a proposed Decentralised Energy Network (DEN) and a Combined Heat and Power (CHP) plant would not be appropriate for this site.

Carbon Offset Contribution

- 6.8.4 New development is expected to achieve the necessary energy and CO2 requirements within the London Plan and Haringey Council's Local Plan or pay an offset payment at £95/tCO2 over 7 years. The applicant has submitted a Carbon Reduction Statement which states that the Carbon shortfall to offset is 11.21 tCO2 which requires a carbon offset contribution of £7,454.65. This will be required by Condition.

Adherence to submitted Energy Strategy

- 6.8.5 Conditions are attached to ensure that the development shall be constructed in strict accordance with the Carbon Reduction Statement, Technical Information Report, and Overheating Study. This shall ensure that the agreed carbon reduction of 59% beyond a Building Regulations 2013 compliant building (with SAP10 carbon factors) will be achieved. The condition would also ensure that the energy efficient materials and air source heat pumps are maintained for the lifetime of the development.

6.9 Pollution – Air quality & contamination

- 6.9.1 Policy 5.21 (Contaminated land) of the London Plan (2016) highlights the Mayor's support for the remediation of contaminated sites and commits him to work with strategic partners to ensure that development of brownfield land does not result in significant harm to human health or the environment and to bring contaminated land to beneficial use.
- 6.9.2 DMDPD Policy DM23: Environmental Protection notes that proposals for new development will only be permitted where it is demonstrated that any risks associated with land contamination, including to human health and the environment, can be adequately addressed in order to make the development safe.
- 6.9.3 It goes on to state that all proposals for new development on land which is known to be contaminated, or potentially contaminated, will be required to submit a preliminary assessment to identify the level and risk of contamination and, where appropriate, a risk management and remediation strategy.

Land Contamination

- 6.9.4 The applicant has submitted information in this regard which has been assessed by the Council's Lead Officer – Pollution. They have no objection to the development in relation to Land Contamination but recommend a condition which would require a site investigation to be designed for the site which would enable

a risk assessment to be undertaken and a Method Statement to be developed which details remediation requirements. The condition would then require these to be carried out should they be required.

- 6.9.5 A further condition is recommended which would require development works to stop should any contamination not previously identified be found to be present at the site. A remediation strategy detailing how this contamination would be dealt with would then need to be submitted and approved in writing by the Local Planning Authority and implemented as approved.

Air Quality

- 6.9.6 Policy SI 1 Improving air quality sets out several criteria to tackle poor air quality, protect health and meet legal obligations. Policy DM23: Environmental Protection of the DMDPD requires all development proposals to consider air quality and be designed to improve or mitigate the impact on air quality in the Borough; and improve or mitigate the impact on air quality for the occupiers of the building or users of the development.
- 6.9.7 The Council's Lead Officer – Pollution has no objection to the development in relation to air quality. However, to safeguard residential amenity, reduce congestion, and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality a condition is recommended which would require a Construction Environmental Management Plan (CEMP) to be submitted and approved in writing by the local planning authority.

6.10 Flood Risk and Drainage

- 6.10.1 Publication London Plan Policies SI 12, SI 13 and Local Plan (2013) Policy SP5 (Water Management and Flooding) require developments to utilise Sustainable Urban Drainage Systems (SUDS) unless there are practical reasons for not doing so, and aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible in line with the drainage hierarchy.
- 6.10.2 Policy also requires drainage to be designed and implemented in ways that deliver other policy objectives, including water use efficiency and quality, biodiversity, amenity, and recreation. Further guidance on implementing Policy 5.13 is provided in the Mayor's Sustainable Design and Construction SPG (2014) including the design of a suitable SUDS scheme.
- 6.10.3 The site is located within Flood Zone 1 (low risk) but is within a Critical Drainage Area. The applicant has submitted a Drainage and Flood Risk Design Note and a Below Ground Drainage Layout drawing that includes an attenuation system and permeable paving throughout the site.

- 6.10.4 The Council's SuDS Officer has been consulted and has advised that the LLFA, accept the proposed SuDS solutions. They are satisfied with the proposal in terms of drainage subject to a condition to ensure a revised FRA is submitted that meets all of the requirements of the Haringey guidance as well as evidence that Thames Water have given consent to connect to their network and capacity exists to receive the surface water.
- 6.10.5 A comprehensive management maintenance schedule has been provided that will see the system function effectively for the duration of the site. Conditions are attached to ensure these drainage solutions are delivered and maintained accordingly
- 6.10.6 The Environment Agency have reason to believe the site is within 3 metres of a culverted main river, the Stonebridge Brook, and have raised an objection given this close proximity.
- 6.10.7 The applicant has employed consultants to survey the area who have only located a Thames Water surface water sewer network which is 23.9m away at its closest point. The survey information indicates that no other subterranean waterways have been located closer to the boundary. The applicant has contacted Thames Water to see if they can confirm that the sewer is the adopted watercourse – A response to this has not yet been received.
- 6.10.8 At the time of drafting the report the EA have not retracted their objection. However, the applicant has surveyed the area and cannot locate any waterways. Given the nature of the development – which requires little in the way of excavation and foundations; and given the lack of evidence to counter the applicant's position – there is no evidence to show that the units would be placed within 8m of a culvert. Further information will be provided in an addendum report.

6.11 The impact on the adjacent ecological asset

- 6.11.1 Policy G5 of the Publication London Plan states that major development proposals should contribute to the greening of London by including urban greening as a fundamental element of building design. Policy G6 of the same document states that SINC's should be protected.
- 6.11.2 Policy SP13 of the Local Plan states that all development shall protect and enhance sites of biodiversity and nature conservation. Policy DM19 Part A of the DM DPD states that development proposals which are on or adjacent to a SINC or an ecological corridor should protect and enhance the nature conservation value of the site.
- 6.11.3 Part B of the same policy states that development that has a direct or indirect adverse impact upon important ecological assets will only be permitted where: (a)

the harm cannot be reasonably avoided, and; (b) it has been suitably demonstrated that appropriate mitigation can address the harm caused.

6.11.4 Policy DM21 of the DM DPD states that proposals should maximise opportunities to enhance biodiversity on site, including through appropriate landscaping, living roofs and green walls. Policy DM1 states that development proposals will be expected to respond to trees on or close to the site.

6.11.5 The Council's Nature Conservation Officer has been consulted and is satisfied with the proposal noting that an ecological review of the site has been carried out which has highlighted that there is negligible ecological interest on the site and no remaining scope for protected species to use the site in any significant way.

6.11.6 Adjacent to the site bats are known to be present foraging within the Tottenham Railsides SINC. As such, ensuring the risk of light spill onto the SINC is minimised by careful placement of any external lighting and directional shading would help to protect this European protected species. A condition is attached to ensure harmful light spill into/onto the SINC would not occur.

6.11.7 The Nature consideration officer also notes that the SINC is also directly adjacent and young and semi-mature trees are close to the site. Care must therefore be taken not to damage the root zones or the canopies of these trees (including through soil compaction and disturbance).

6.11.8 They recommend conditions that would ensure that tree root zones and canopies are protected. They note that a robust fence has been incorporated into the scheme which would ensure access to the SINC is restricted. This would help to preserve and safeguard the ecological asset.

6.11.9 The proposal would therefore protect the ecology of the area subject to the imposition of the conditions mentioned above.

6.12 Waste and Recycling

6.12.1 London Plan Policy 5.16 indicates the Mayor is committed to reducing waste and facilitating a step change in the way in which waste is managed. Local Plan Policy SP6 Waste and Recycling and DPD Policy DM4 require development proposals to make adequate provision for waste and recycling storage and collection.

6.12.2 The proposal provides sufficient waste storage through the provision of a refuse store which holds 5 x 1100L waste receptacles for refuse & 3 x 1100L waste receptacles for dry recycling. The plans indicate that the waste containers would be located no further than 10 metres from the point of collection (which would be on Ermine Road).

- 6.12.3 The plans also show the route from the waste storage point to the collection point to be as straight as possible with no kerbs or steps. The gradient would also be close to 1:20 and surfaces would be smooth and sound, & concrete rather than flexible. A dropped kerb is already installed, in the location where they would be placed onto the street for collection.
- 6.12.4 Generally, major schemes with this many units would provide for waste collection vehicles to enter and exit the development using forward motion gears. The proposed development would have a modest floorspace and would be on a small site at the end of what is effectively a cul-de-sac. The specialist nature of the housing and the characteristics of the locality do not require this. Provision would be made for waste receptacles to be collected by a refuse vehicle in Ermine Road.
- 6.12.5 A condition is attached which would require the codes, keys, transponders or any other type of access equipment to be provided to the Council and for the access to the waste storage to remain unobstructed and within 10m from Ermine Road on collection day. This would ensure suitable provision is provided.

6.13 Conclusion

- 6.13.1 The proposals would provide much needed housing for a section of the community where the need is amongst the greatest. The site is suitable for residential development, but this would be unlikely to come forward in another form due to it being safeguarded for Crossrail 2. The proposal would provide an exemplary modular building, in a form that is of gentle density and appearance that would be compatible with the varied but mostly residential context.
- 6.13.2 These homes would be steppingstones to help people transition from rough sleeping. The proposals would give prospective residents time to adjust to living in their own place and to develop their independent living skills. Following this they will move on to their own permanent homes. The units are thoughtfully designed through consultation with individuals who have slept rough or have had experiences of homelessness.
- 6.13.3 The GLA, through the Rough Sleeping Accommodation Programme (RSAP), has awarded Haringey Council funding for the scheme, based on the current size of the proposed units. The sizes not only aid delivery but prevent cuckooing and the renting out of bedrooms for other antisocial purposes.
- 6.13.4 The proposal and the recommended conditions ensure the proposal delivers much needed temporary housing in a way that complies with the development plan. The concerns of residents are noted but the submission outlines how these issues would be minimised and addressed to ensure the safety of new and existing residents.

6.13.5 All other relevant policies and considerations have been taken into account.

Equalities

6.13.6 In determining this planning application, the Council is required to have regard to its obligations under equalities legislation including obligations under the Equality Act 2010. In carrying out the Council's functions due regard must be had, firstly to the need to eliminate unlawful discrimination, and secondly to the need to promote equality of opportunity and to foster good relations between persons who share a protected characteristic and persons who do not share it.

6.13.7 As discussed above, the development provides accommodation and access to support for people who are street-homeless. This will also likely provide other opportunities that street-homeless people may not currently have equal access to. It is also noted that street-homelessness is more likely to affect groups with a protected characteristic such as individuals with disabilities, those experiencing mental health issues, LGBTQ+ persons, and young men. The proposal would support equality of opportunity and may also benefit specific priority groups.

6.13.8 Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION

6.14 CIL

6.14.1 Based on the information given on the plans, the Mayoral CIL charge will be £54,720 (912sqm x £60.55) and the Haringey CIL charge will be £19,061 (912sqm x £20.90). This would be charged in accordance with the CIL Charging Schedule and any eligible relief and includes indexation in line with the RICS CIL Index.

7 RECOMMENDATIONS

GRANT PERMISSION subject to conditions in Appendix 1

Applicant's drawing No.(s)

Location Plan 109-08-PS-001; Proposed Site Plan 109-08-PS-002 K; Proposed Ground Floor Plan 109-08-PS-010 K; Proposed First Floor Plan 109-08-PS-011; Block A Elevations / Street Section 109-08-PS-020 Rev: C; Block B Elevations / Site Section A 109-08-PS-021 Rev: C; Block C Elevations / Site Section B 109-08-PS-022 Rev: C; GA Elevations (Colour) 0000-HF-000-00-DR-BC-0002 REV C03; ROOF PLAN CHM-D-18 D04; GENERAL PLAN CHM-D-01 D05

Appendix 1

Subject to the following condition(s)

Temporary Permission

- 1) This permission shall be for a limited period expiring on 15/01/2028 when the building hereby approved shall be removed and the land reinstated.

Reason: The permanent retention of the building may prejudice the future development of the site thus preventing the optimal use of the site inconsistent with Policy 3.4 of the London Plan 2016.

Approved Plans

- 2) The approved plans comprise drawing numbers (Location Plan 109-08-PS-001; Proposed Site Plan 109-08-PS-002 K; Proposed Ground Floor Plan 109-08-PS-010 K; Proposed First Floor Plan 109-08-PS-011; Block A Elevations / Street Section 109-08-PS-020 Rev: C; Block B Elevations / Site Section A 109-08-PS-021 Rev: C; Block C Elevations / Site Section B 109-08-PS-022 Rev: C; GA Elevations (Colour) 0000-HF-000-00-DR-BC-0002 REV C03; ROOF PLAN CHM-D-18 D04; GENERAL PLAN CHM-D-01 D05). The development shall be completed in accordance with the approved plans and retained as such thereafter for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: In order to ensure the development is carried out in accordance with the approved details and in the interests of amenity and good planning.

Materials as indicated on approved plans

- 3) The external materials to be used for the proposed development shall match the colour, size, shape, and texture of the materials indicated on the approved plans except where conditions attached to this planning permission indicate otherwise.

Reason: In order to ensure a satisfactory appearance for the proposed development, to safeguard the visual amenity of neighbouring properties and the appearance of the locality consistent with Policy 7.6 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2013 and Policy DM1 of The Development Management DPD 2017.

No Parking Permits for New Residents

- 4) The development shall not be occupied other than through a lease/license/agreement between the operator and each resident that ensures that occupants of the development hereby approved are obligated not to apply for a parking permit for any Controlled Parking Zone (CPZ).

Reason: In order to ensure car parking is restricted in line with levels of existing and future public transport accessibility and connectivity and to comply with Policy T6 of the Publication London Plan (2020) & Policy DM32 of The Development Management DPD 2017.

All Parking Delivered as Approved

- 5) The vehicular and cycle parking spaces shown on the approved plans shall be laid out and installed as approved and retained for the lifetime of the development. The hatched parking space indicated on drawing '109-08-PS-010 K' shall only be used for deliveries & servicing; or in/for emergencies except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: In order to ensure sufficient space is given over within the development to facilitate safe, clean, and efficient deliveries and servicing and to comply with Policies T5, T6, T6.1, & T7 of the Publication London Plan (2020).

Construction Management/Logistics Plan

- 6) Prior to above ground works, a Construction Management/Logistics Plan shall be submitted to and approved in writing by the local planning authority. The CLP shall be in accordance with Transport for London's Construction Logistics Plan Guidance (July 2017) and shall provide details on:
- i. Details of key phases of the construction programme, corresponding site layout, number of staff including modes of travel, parking provision (vehicles and cycles);
 - ii. Arrangements for management of construction material deliveries / removal, material storage, skip storage;
 - iii. Daily number and size of construction vehicles expected, vehicular swept paths (with 300mm error margins) to demonstrate largest construction vehicles arriving, parking in loading/unloading area, manoeuvring and departing in forward gear;
 - iv. Details of all temporary traffic management and parking restrictions required;
 - v. Details of any highway licences required due to the crane oversailing the public highway;
 - vi. Wheel washing facilities to keep highway clean of mud etc;
 - vii. Arrangements for management of Health and safety;
 - viii. Arrangements for dealing with complaints;
 - ix. Hours of operations;
 - x. Monitoring and joint working arrangements, where appropriate;
 - xi. Site access and car parking arrangements;
 - xii. Delivery booking systems (allocated delivery slots for site management);

- xiii. A swept path analysis for crane vehicles carrying the necessary equipment;
- xiv. Agreed routes to/from the site;
- xv. Confirmation that all vehicles are recognised in the Fleet Operators Recognition Scheme (FORS) or similar;
- xvi. Timing of deliveries to and removals from the site by construction vehicles (to avoid peak times, as agreed with Highways Authority, 07.00 to 9.00 and 16.00 to 18.00, where possible); and
- xvii. Travel plans for staff/personnel involved in construction works to detail the measures to encourage sustainable travel to the site during the construction phase; and
- xviii. Joint arrangements with neighbouring developers for staff parking, Lorry Parking, and consolidation of facilities such as concrete batching; and
- xix. Temporary obstructions during the construction and delivery must be kept to a minimum and should not encroach on the clear space needed to provide safe passage for pedestrians or obstruct the flow of traffic on the TLRN.

Reason: In order to ensure that the proposal is consistent with Publication London Plan Policy T7 and to safeguard residential amenity, reduce congestion and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality and to comply with DMDPD Policy DM23: Environmental Protection.

Energy Strategy

- 7) The development shall be constructed in accordance with:
- the Carbon Reduction Statement – ModulHaus Ermine Road prepared by Volumetric Modular Ltd (dated December 2020);
 - Overheating, MVHR, ASHP Technical Information Report prepared by Hill (dated 5 October 2020); and
 - the TM59 Overheating Study prepared by The Richards Design Partnership (dated November 2020).

and

the energy efficient materials and air source heat pumps shall be maintained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: To ensure the development can comply with the Energy Hierarchy in line with London Plan 2016 Policy 5.2, Publication London Plan Policy SI 2 and Local Plan Policy SP4.

Carbon Offset Contribution

- 8) The development hereby approved shall not be occupied until a payment of £7,454.65 towards carbon reduction measures in Haringey has been paid to the Council's Carbon Management Team to bring the level of carbon reduction from the site in line with the London Plan Policies 5.1, 5.2, 5.3, 5.7, 5.8, 5.9, 5.10, and 5.11; The Publication London Plan (2020) Policy SI 2; and Local Plan Policy SP4.

Reason: To ensure the development can comply with Publication London Plan Policy SI2 and Local Plan Policy SP4.

Land Contamination

- 9) Prior to any further work on site:
- a) Using the information already acquired from the submitted Phase I Geo-Environmental Assessment with reference DS-21906G-20-420 proposed by IDOM Merebrook Limited dated October 2020, a site investigation shall be designed for the site using information obtained from the desktop study and Conceptual Model. The site investigation must be comprehensive enough to enable; a risk assessment to be undertaken, refinement of the Conceptual Model, and the development of a Method Statement detailing the remediation requirements.
 - b) The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority which shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site.
 - c) Where remediation of contamination on the site is required, completion of the remediation detailed in the method statement shall be carried out and;
 - d) A report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority before the development is occupied.

Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety and to comply with London Plan (2016) policy 5.21 (Contaminated land) and DMDPD Policy DM23.

Unexpected Contamination

- 10) If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until a remediation strategy detailing how this contamination will be dealt with has been submitted to and approved in writing by the Local Planning Authority. The remediation strategy shall be implemented as approved.

Reason: To ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels of water pollution from previously unidentified contamination sources at the development site in line with paragraph 109 of the National Planning Policy Framework.

CEMP

- 11) Prior to above ground works, a Construction Environmental Management Plan (CEMP) shall be submitted to and approved in writing by the local planning authority.

The following applies to above:

- a) The CEMP shall include an Air Quality and Dust Management Plan (AQDMP).
- b) The CEMP shall provide details of how construction works are to be undertaken respectively and shall include:
 - i. A construction method statement which identifies the stages and details how works will be undertaken;
 - ii. Details of working hours, which unless otherwise agreed with the Local Planning Authority shall be limited to 08.00 to 18.00 Monday to Friday and 08.00 to 13.00 on Saturdays;
 - iii. Details of plant and machinery to be used during demolition/construction works;
 - iv. Details of an Unexploded Ordnance Survey;
 - v. Details of the waste management strategy;
 - vi. Details of community engagement arrangements;
 - vii. Details of any acoustic hoarding;
 - viii. A temporary drainage strategy and performance specification to control surface water runoff and Pollution Prevention Plan (in accordance with Environment Agency guidance);
 - ix. Details of external lighting; and,
 - x. Details of any other standard environmental management and control measures to be implemented.
- c) The AQDMP will be in accordance with the Greater London Authority SPG Dust and Emissions Control (2014) and shall include:
 - i. Mitigation measures to manage and minimise demolition/construction dust emissions during works;
 - ii. Details confirming the Plot has been registered at <http://nrmm.london> where applicable;
 - iii. Evidence of Non-Road Mobile Machinery (NRMM) and plant registration shall be available on site in the event of Local Authority Inspection;
 - iv. An inventory of NRMM currently on site (machinery should be regularly serviced, and service logs kept on site, which includes proof of emission limits for equipment for inspection);
 - v. A Dust Risk Assessment for the works; and
 - vi. Lorry Parking, in joint arrangement where appropriate.

Reason: To safeguard residential amenity, reduce congestion and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality and to comply with Policy 5.21 (Contaminated land) of the London Plan (2016) and DMDPD Policy DM23: Environmental Protection.

Drainage / Flooding (FRA)

- 12) Prior to above ground works, a revised FRA shall be submitted to and approved in writing by the local planning authority. The revised FRA shall provide new rainfall data using Flood Estimation Handbook (FEH) rainfall theory, in accordance with Haringey guidance.

The FRA shall include a completed London Sustainable Drainage pro-forma, as well as evidence that Thames Water have given consent to connect to their network and capacity exists to receive the surface water.

Reason: In order to comply with Policies DM24, 5, 6, & 9 of the Haringey Development Management DPD (2017), policy SP5 of the Local Plan.

Provision & Retention of SuDS

- 13) Prior to above ground works, Section 4 (Sustainable Drainage Maintenance Strategy) & Appendix B (Proposed Drainage Strategy Drawings) of 'Design Note – Ermine Rd Drainage & Flood Risk 16/12/2020' and the SuDS options it proposes (namely Below Ground Cellular Attenuation & Permeable Paving) shall be provided, maintained in accordance with the associated Maintenance Schedules, and retained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

For the avoidance of doubt, all hardstanding areas shall be permeable.

Reason: In order to ensure a satisfactory provision for drainage on site and ensure suitable drainage provision for the development and comply with Policies DM24, 5, & 6 of the Haringey Development Management DPD (2017), policy SP5 of the Local Plan.

No Light Spill into SINC (Bats)

- 14) Light from external lighting in the development shall not spill to a harmful extent into the adjacent ecological asset (Tottenham Railsides SINC). Any lighting located near the northwest boundary of the site shall have directional shading to ensure that light spillage into the SINC does not have an undue impact on foraging bats and their habitats.

Reason: In order to ensure bats and their habitats are suitably protected and comply with Policies DM19 (Nature Conservation) of the Haringey Development

Management DPD (2017), policy 7.19 of the London Plan, and Policy G6 of the Publication London Plan.

Tree protection (SINC/Bats)

- 15) The existing trees adjacent to the northwest boundary of the site shall not be lopped, felled or otherwise affected in any way (including raising and lowering soil levels under the crown spread of the trees) and no excavation shall be cut under the crown spread of the trees without the prior written permission of the Local Planning Authority.

Reason: In order to ensure bats and their habitats are suitably protected and to safeguard trees in the interest of visual amenity of the area consistent with Policy 7.21 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2017 and Policies DM1 and DM19 of The Development Management DPD (2017).

Waste storage delivered and made accessible

- 16) The waste storage shown on the approved plans shall be delivered and retained for the lifetime of the development except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

On occupation of the development - the codes, keys, transponders or any other type of access equipment to the waste store shall be provided to the Council and access to the waste store shall remain unobstructed and within 10m from Ermine Road on collection day.

Reason: In order to ensure waste storage is suitably accessible and to comply with Policy DM4 of The Development Management DPD (2017).

Secured by Design

- 17) Prior to occupation of the development, details of full Secured by Design' Accreditation shall be submitted to and approved in writing by the Local Planning Authority. The details shall demonstrate consultation with the Metropolitan Police Designing Out Crime Officers. The development shall be carried out in accordance with the approved details and maintained thereafter for the lifetime of the development.

Reason: To ensure safe and secure development and reduce crime and to comply with Publication London Plan Policy D11 and DMDPD Policy DM2.

Culvert protection

- 18) If, during development, a culverted main river previously not identified is found to be present under, or within 4m of, the site then no further development shall be carried out until a condition survey has been completed and a strategy put in

place to protect the culvert. The strategy will be submitted to and approved in writing by, the local planning authority (in consultation with the Environment Agency), and shall be implemented as approved. The strategy will include the following components;

- Details of the location, depth, and structural condition of the culvert.
- Plans to repair any damage the culvert to ensure structural stability for the lifetime of the development.
- Details of how work will be carried out on site without damaging the structure, or impeding the function of, the culvert (including loading calculations).

Reason: To reduce the risk of flooding to the proposed development and future occupants, ensure the structural integrity of the existing flood defences and reduce the risk of flooding from blockages to any existing culvert in accordance with Policy DM28: Protecting and Enhancing Watercourses and Flood Defences.

Informatives:

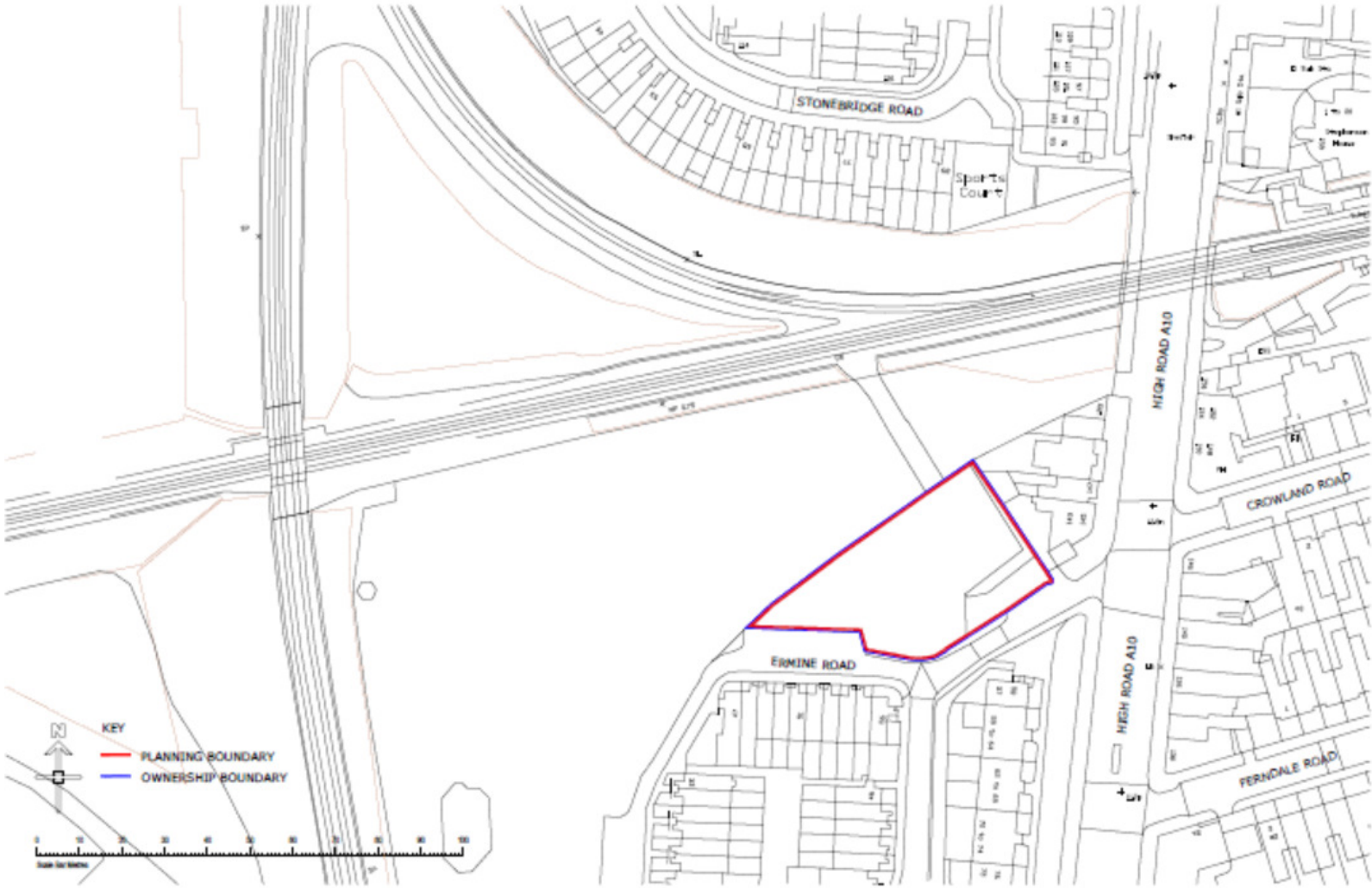
- 1) **INFORMATIVE :** In dealing with this application, Haringey Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) (Amendment No.2) Order 2012 to foster the delivery of sustainable development in a positive and proactive manner.
- 2) **INFORMATIVE : CIL**
Based on the information given on the plans, the Mayoral CIL charge will be £54,720 (912sqm x £60.55) and the Haringey CIL charge will be £19,061 (912sqm x £20.90). This would be charged in accordance with the CIL Charging Schedule and any eligible relief and includes indexation in line with the RICS CIL Index.
- 3) **INFORMATIVE : Hours of Construction Work:** The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:-
 - 8.00am - 6.00pm Monday to Friday
 - 8.00am - 1.00pm Saturday
 - and not at all on Sundays and Bank Holidays.
- 4) **INFORMATIVE : Party Wall Act:** The applicant's attention is drawn to the Party Wall Act 1996 which sets out requirements for notice to be given to relevant adjoining owners of intended works on a shared wall, on a boundary or if excavations are to be carried out near a neighbouring building.
- 5) **INFORMATIVE :** The new development will require numbering. The applicant should contact the Local Land Charges at least six weeks before the development is occupied (tel. 020 8489 5573) to arrange for the allocation of a suitable address.
- 6) **INFORMATIVE :** The London Fire Brigade strongly recommends that sprinklers are considered for new developments. Sprinkler systems installed in buildings can significantly reduce the damage caused by fire and the consequential cost to businesses and housing providers, and can reduce the risk to life. The Brigade opinion is that there are opportunities for developers and building owners to install sprinkler systems in order to save money, save property and protect the lives of occupier.
- 7) **INFORMATIVE :** With regards to surface water drainage, it is the responsibility of a developer to make proper provision for drainage to ground, water course, or a suitable sewer. In respect of surface water, it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to

a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.

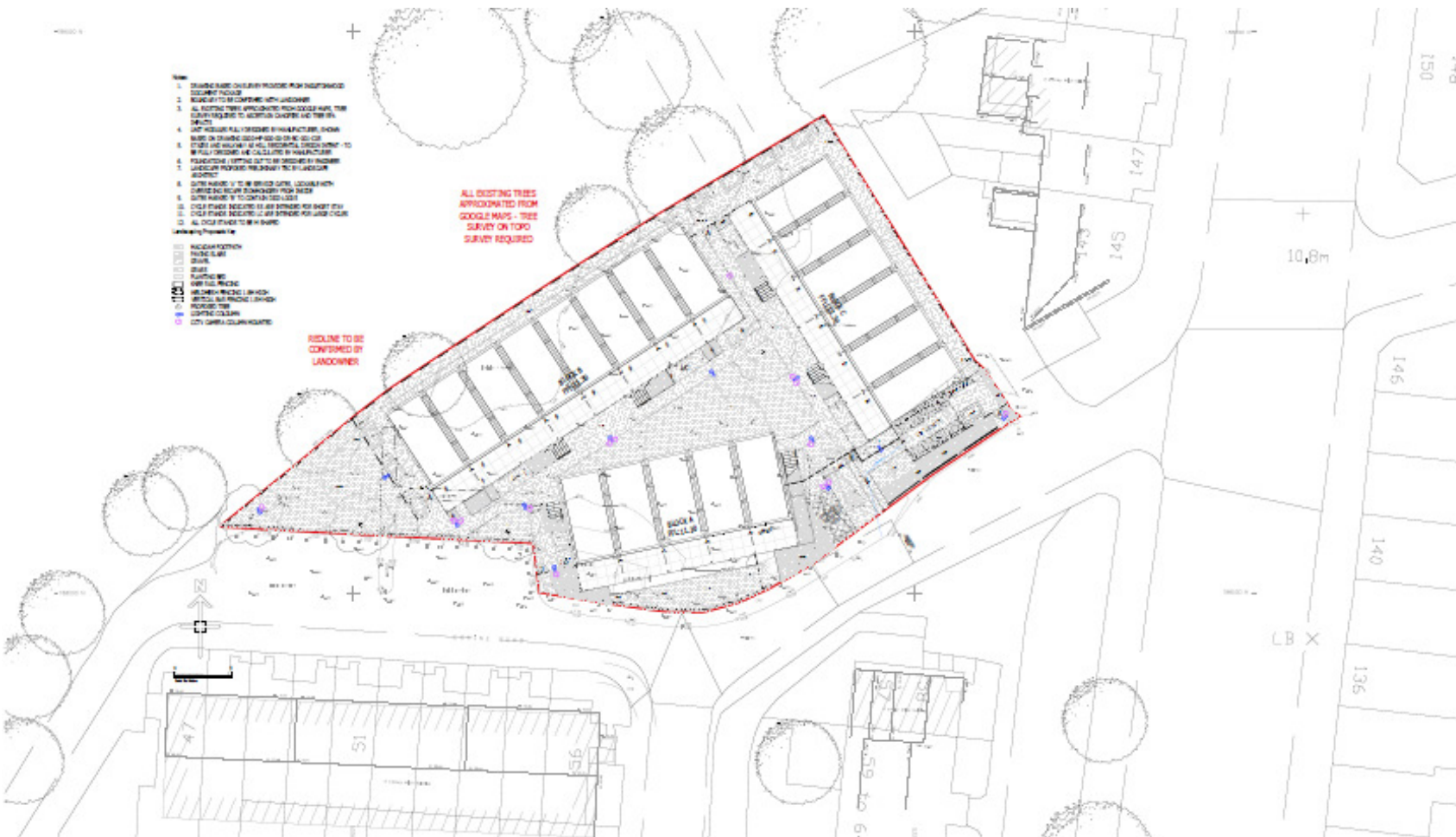
- 8) INFORMATIVE : Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Water's pipes. The developer should take account of this minimum pressure in the design of the proposed development.
- 9) INFORMATIVE : A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed online via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.
- 10) INFORMATIVE : The proposed development is located within 15 metres of Thames Water's underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures (<https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>). Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB
- 11) INFORMATIVE : The applicant should be aware that the application site is identified as an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Directions. In the event of a decision to progress the Crossrail 2 project the land may be subject to compulsory purchase in order to provide a worksite for the future delivery of the Crossrail 2 scheme.
- 12) INFORMATIVE : The applicant must seek the continual advice of the Metropolitan Police Service Designing Out Crime Officers (DOCOs) to achieve accreditation. The services of MPS DOCOs are available Free of Charge and can be contacted via docomailbox.ne@met.police.uk or 0208 217 3813.

Appendix 2 Plans and Images

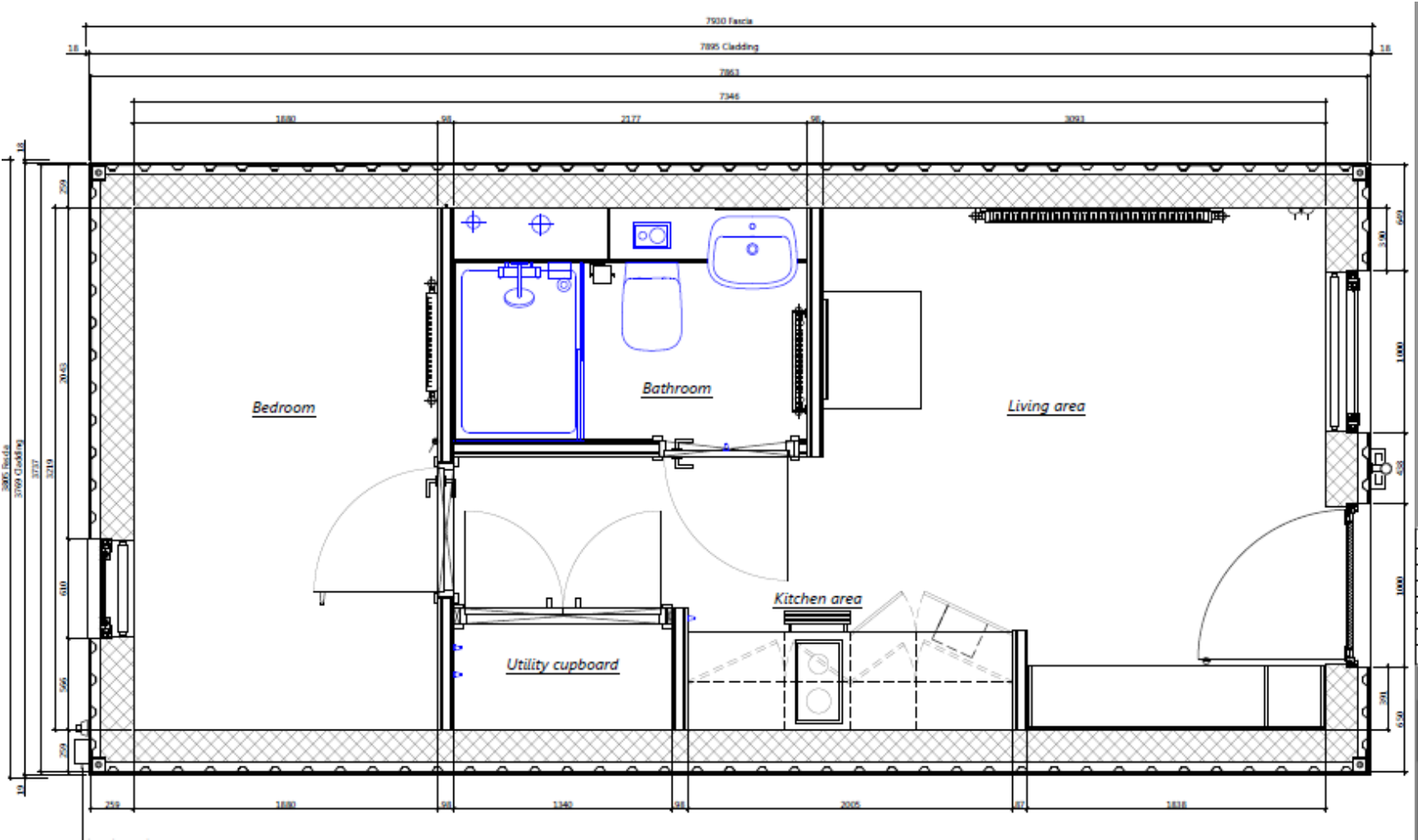
Location Plan



Site Plan



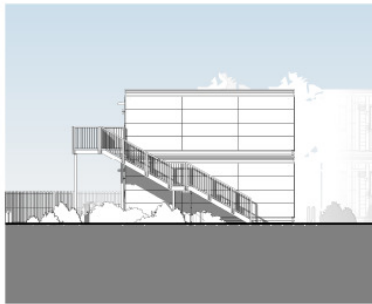
Indicative room layout



Block A Elevation



① Block A Front Elvation / Ermine Rd Section
1 : 100



② Block A Right Elevation
1 : 100



③ Block A Rear Elevation
1 : 100



④ Block A Left Elevation
1 : 100



Client:
140 Residential | Irvine

Block B



① Block B Front Elevation / Site Section A
1 : 100



② Block B Right Elevation
1 : 100



③ Block B Rear Elevation
1 : 100



④ Block B Left Elevation
1 : 100

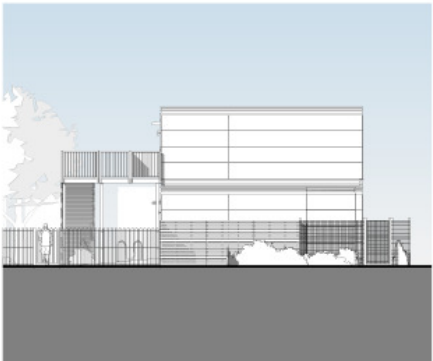


Drawn
1:100 (Front elevation and 1:1000 plan)

Block C



① Block C Front Elevation / Site Section B
1 : 100



② Block C Right Elevation
1 : 100



③ Block C Rear Elevation
1 : 100



④ Block C Left Elevation
1 : 100

Render of view from Crowland Road / High Road



View from Ermine Road looking East



View from Ermine Road looking North



View from High Road looking northwest



Appendix 3 Consultation Responses from internal and external agencies

Stakeholder	Question/Comment	Response
INTERNAL		
Design	<p><u>HGY/2020/2794</u> - Land to the North of Ermine Road, London N15 <i>Temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as accommodation for the homeless with associated cycle storage and refuse storage.</i></p> <p>Thank you for asking me for my comments on the above. I have been included in discussions on this proposal since the pre-app stage and am familiar with the site after discussing earlier proposals including the outline permission granted for residential development on the neighbouring green space.</p> <p>The site is an area of hardstanding located between commercial properties facing Tottenham High Road to the east, the Ermine Road estate, 1970s council housing, to the south, and the Ermine Triangle green space to the north-west. This is an area of leftover land originally intended, when the railways were built in the late nineteenth century, for a connecting rail loop between two crossing railways (both now parts of the London Overground passenger network); the Liverpool Street to Enfield Town line about 100m to the west and the Gospel Oak to Barking line about 40m to the north. No proposals have ever been made for constructing such a loop, nor are any likely, as such a link would not be necessary. There is already a link from the Barking line to the Enfield Town line, used for only occasional freight.</p> <p><u>Principle of Development</u></p> <p>The green space adjoining the application site to the north-west, along with the railway tracks an similar green triangles on the other sides of both tracks, have been designated for nature conservation in Haringey's Local Plan, as Ecological Corridor and Site of Importance for Nature Conservation (SINC) Grade II, but these designations do not apply to this application site, merely to the land adjoining its north-western boundary. There are also no</p>	<ul style="list-style-type: none"> Noted

Stakeholder	Question/Comment	Response
	<p>other planning designations applicable to this site or any of its immediate surroundings, not is the site designated for development. It is, however, considered a potentially developable piece of brownfield land subject to normal planning policies applicable to any other developable plot. The neighbouring greenspace is privately owned and is in principle developable, provided the biodiversity value can be maintained, and outline planning permission has previously been granted (on appeal) for a modest residential development on the part of the site near this application site. There is also a small plot of land, currently in use as a vehicle mechanics, at the back of the nearest property on Tottenham High Road, immediately adjoining the north-eastern side of the application site, separating it from the shops, with flats above, in Victorian three storey sopping parade buildings facing the High Road.</p> <p>The proposals are for temporary housing using units manufactured off-site, in modular, self contained, single bedroom, single person units. The proposed housing will act as a first step for homeless people, with on site management, advice and assistance. The site is suitable for residential development in principle, adjoining existing housing to the south, and has the facilities and connections available on Tottenham High Road just to the east with Seven Sisters Station and the West Green Road town centre a short walk to the north, and South Tottenham Overground Station an even shorter walk just across the High Road, which make it particularly suited to specialist, supported housing of this sort. However, the site is only available for temporary development, being safeguarded for future use to construct Crossrail 2, and the modular, off-site manufactured design is suitable for dismantling and relocating to another site should Crossrail (or another proposal) go ahead requiring access to the development. Off-site manufactured buildings are probably the only sorts of technology that could be used to build on this site whilst allowing for its to be vacated in only a short number of years, with the investment in essential housing not being lost.</p> <p>Form & Pattern of Development</p>	

Stakeholder	Question/Comment	Response
	<p>The form of the proposal is in many ways driven by the progressive construction concept of Off-Site Manufacture and Assembly, with the development designed to be largely manufactured as a series of “pods” in a factory, each the size of the individual one bedroom units. These will be delivered to site and joined together in three, two storey rows of five, six and eight units each, giving 37 one-bed flats and one office. Each has a balcony access to the upper floor flats; that also provide a sheltered outdoor threshold space to the ground floor flats beneath them. The rows are laid out around the edge of the site, creating a central communal amenity and outdoor meeting place, well overlooked by two storeys of flats on two sides; the rows on the north-east and north-west side (Blocks B and C respectively) back onto their boundaries with the private sides of these neighbours, whilst the front Block A faces the street, Ermine Road, with its’ back facing the central communal space.</p> <p>Paths, entrances and site enclosure reinforce this simple layout to create a clear and logical spatial hierarchy, from the public street frontage, with just four of the flats opening off this, as well as the site office, but with the five upper floor flats of Block A also looking onto this, with a clear single gate-controlled entrance, located between Blocks A and B, beside the site office. Block A is modestly set back from the pavement edge along Ermine Road (which itself would be improved from the narrow, inconsistent present pavement), with a zone of defensible space between the flats and the street, but this will make a significant contribution to improving animation and liveliness to the street. The rest of the site layout and enclosure will provide clear, secure boundaries, avoiding creating unobserved ambiguous corners and providing a clear pedestrian route from street to site entrance and site entrance to flat entrance, with stairs to access balconies to 1st floor flats all opening off and clearly visible from the central space. Services, storage and refuse are located conveniently but unobtrusively to reinforce the clear, logical and secure site layout.</p> <p><u>Bulk, Massing & Landscaping</u></p>	

Stakeholder	Question/Comment	Response
	<p>The proposals' height is limited to two storeys and the length of the three blocks limited to five, six and eight units, broken down by a clear rhythm into short units, make it clearly within and below the prevailing height and bulk of the surroundings. The central space will provide a convivial communal meeting and amenity space, benefiting from grassed and paved areas to provide a variety of external amenity in addition to the "stoop" private external amenity outside each flat's front door. The western edge of the site, where it narrows down to an acute angle, will in contrast be landscaped with trees and bushes to compliment and extend the more wild, natural landscaping of the green space of the railway triangle to the north-west.</p> <p><u>Elevational Treatment, Fenestration, including Balconies</u></p> <p>The three rows will have a strongly rhythmic appearance to their main long front and back elevations, formed by their repeating module of windows, and blocks of coloured panels representing each individual flat, contrasting with darker panels between and above flats, which are also used for the whole of the much shorter, blank, end elevations. The rhythm of windows and doors, panels and colours, will provide an attractive elevational / fenestration pattern of a comparable scale and proportion to the ordinary domestic surroundings, whilst being clearly of contemporary appearance expressing its progressive modern construction.</p> <p>Balconies providing access to the first floor flats, as well as their outdoor amenity space, will form an important part of the developments' appearance and add to the liveliness and animation of the scheme. The balconies are over 2m deep, providing space for both circulation and private outdoor amenity. On design officer advice, solid panels have been added to the balcony balustrading, in the same material as the side panelling, to break up the length of the balconies, provide some privacy to first floor residents and hide any clutter on those balconies.</p>	

Stakeholder	Question/Comment	Response
	<p><u>Conclusions</u></p> <p>The proposals will provide much needed housing for a section of the community where the need is amongst the greatest, in a site for which residential development is suitable, but that is otherwise in danger of being blighted by its Crossrail 2 Safeguarding. It also provides an exemplary example of Modern Methods of Construction, off site manufacture and pre-fabrication, in a form that is of gentle density and appearance, compatible with the varied but mostly residential context. The design will provide a secure, safe and clearly defined boundary and definition of public, private communal and private space, with active street frontage. Green landscaping proposed for the site will soften its appearance and be compatible with the neighbouring site of nature importance.</p>	
Conservation	<p><u>HGY/2020/2794 - Land north of Ermine Road, N15 6DD - Update</u></p> <p>The proposed development falls within the setting of the Seven Sisters/Page Green Conservation Area. The submitted image shows the proposed development from the corner of Crowland Road and the High Road, opposite (south) of the locally listed Dutch House and just outside the conservation area.</p> <p>The proposed development will only be partially visible from the southernmost part of the conservation area, as it will be mostly screened by the terrace on the western side of the High Road. Due to its mass and scale, it will be consistent with the surrounding townscape and will not detract from it. Therefore, it will not have a negative impact on the significance of the conservation area and associated heritage assets, including the locally listed Dutch House.</p> <p>There is no objection from a conservation perspective. However, it would be desirable the materials and colour palette of the development to reflect materials used in the local area, rather than contrast with them. In addition, there is an opportunity to create an</p>	<ul style="list-style-type: none"> • No harm has been identified and the scheme is therefore acceptable in this regard. • The Conservation Officer has noted that it would be desirable for the materials and colour palette of the development to reflect materials used in the local area, rather than contrast with them. However, this method of construction would not allow brick or a brick cladding panel to be used. Furthermore, the scale of the building would be modest and its siting (with the larger two blocks set back in the site) would mean that the building would have an acceptable appearance.

Stakeholder	Question/Comment	Response
	attractive and carefully designed frontage onto Ermine Road and that could be further explored.	
Transportation	<p>I would suggest condition for the following:</p> <ol style="list-style-type: none"> Construction Management Plan (CMP) – this will help resolve issues raised in my comments regarding CLP in email below. Deliver and Servicing Management Plan – this will help formalise arrangement informed by appropriate vehicular swept paths, accurate location of bollards including arrangements for refuse collection as agreed by Martin Lester. S106 / s278 agreement for highway works – this would help resolve issues raised in my email dated 18/12/20 regarding: <ol style="list-style-type: none"> Car free / car capped s106 agreement to restrict eligibility of all occupiers from obtaining CPZ parking permits and payment (£4000) for amending the CPZ TMO accordingly. highway boundaries – to clearly identify where highway including CS1 may be affected by proposals, 	<ul style="list-style-type: none"> A condition is recommended which would ensure that the development would not be occupied other than through a lease/license that ensures that residents are obligated not to apply for a parking permit for any CPZ. A parking space is provided for deliveries and emergency vehicles and a condition is recommended to ensure it is only used as such. There would be a deficit of 1 space from the minimum which is acceptable given the specialist nature of the housing and given any further provision would not be sheltered and usable.

Stakeholder	Question/Comment	Response												
	<p>iii. status of road along site frontage (Red Route / Highway Authority),</p> <p>iv. relocation / removal of existing bollards (if necessary for parking spaces to operate),</p> <p>v. allowing for highway improvements / reinstatement of redundant crossovers and</p> <p>vi. provision of public footway along site frontage to be designed, funded and implemented, in consultation with TfL.</p> <p>Transport demands and provision for the proposed housing scheme for homeless people are expected to be different from those associated with conventional housing. I also appreciate that there are funding constraints that limit scope of highway works. Under these circumstance, some flexibility could be exercised in provision for car parking, and provision for servicing and deliveries, however, I consider that the proposed development warrants improved provision for footways along the site frontage and this should allow for reinstatement of existing redundant crossovers / dropped kerbs.</p>	<ul style="list-style-type: none"> Construction Management/Logistics Plan required by Condition The proposed development does not propose any highway works. In the absence of any works being made to the highway, a s278 agreement would not be required. A request has also been made for the development to deliver highway improvements and provide a public footway along the site frontage, so that the northern side of Ermine Road has a continuous footway connecting to the High Road. Given the temporary nature of the development the improvements could be superseded after the 7 year period, so it is unreasonable and unnecessary for the development to provide this. 												
Carbon Management (Energy, offset, overheating, sustainability)	<p>Carbon Management Response 16/12/2020</p> <p>On 15/12/2020, the applicant submitted a revised Carbon Reduction Statement – ModulHaus Ermine Road (dated December 2020).</p> <p>It confirms:</p> <ul style="list-style-type: none"> The use of SAP10 and SAP10.1 carbon factors; only SAP10 carbon factors will be used within the reporting. Based on SAP10, the energy hierarchy is: <table border="1"> <thead> <tr> <th></th><th colspan="2">Residential</th></tr> <tr> <th></th><th>tCO₂</th><th>%</th></tr> </thead> <tbody> <tr> <td>Baseline emissions</td><td>27.43</td><td></td></tr> <tr> <td>Be Lean savings</td><td>4.6</td><td>16.81%</td></tr> </tbody> </table>		Residential			tCO ₂	%	Baseline emissions	27.43		Be Lean savings	4.6	16.81%	<ul style="list-style-type: none"> Conditions have been recommended which require this.
	Residential													
	tCO ₂	%												
Baseline emissions	27.43													
Be Lean savings	4.6	16.81%												

Stakeholder	Question/Comment			Response
	Be Clean savings	0	0%	
	Be Green savings	11.6	42.3%	
	Cumulative savings	16.2	59.1%	
	Carbon shortfall to offset (tCO ₂)	11.21		
	Carbon offset contribution	£95 x 7 years x 11.21 tCO ₂ /year = £7,454.65		
	<ul style="list-style-type: none">- A carbon offset contribution of £7,454.65- Energy demand of the development at 22.26 MWh/year- MVHR efficiency of 81%- The ASHPs will not be used for active cooling- ASHP with a SCoP 2.6			
	Proposed Planning Condition			
	<u>Energy Strategy</u>			
	The development shall be constructed in strict accordance with the Carbon Reduction Statement – ModulHaus Ermine Road prepared by Volumetric Modular Ltd (dated December 2020), Overheating, MVHR, ASHP Technical Information Report prepared by Hill (dated 5 October 2020), and the TM59 Overheating Study prepared by The Richards Design Partnership (dated November 2020). This shall include the agreed carbon reduction of 59% beyond a Building Regulations 2013 compliant building (with SAP10 carbon factors). The energy efficient materials and air source heat pumps shall be maintained for the period of 7 years.			
	Confirmation of this must be submitted to the local authority at least 6 months of completion on site for approval and the applicant must allow for site access if required to verify delivery.			
	The Council should be notified if the applicant alters any of the measures and standards set out in the approved energy strategy. Any alterations should be presented with justification and new standards for approval by the Council. Should the agreed target not be able to be achieved on site through energy measures as set			

Stakeholder	Question/Comment	Response
	<p>out in the aforementioned strategy, then any shortfall should be offset at the cost of £2,850 per tonne of carbon.</p> <p>Reason: To ensure the development can comply with the Energy Hierarchy in line with London Plan 2016 Policy 5.2, draft New London Plan (Intend to Publish) Policy SI2 and Local Plan Policy SP4.</p>	
<p>Carbon Management (Pollution – Contamination & Air Quality)</p>	<p><u>Re: Planning Application HGY/2020/2794 on Land to the North of Ermine Road N15</u></p> <p>Thanks for contacting the Carbon Management Team (Pollution) regarding the above planning application for temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as accommodation for the homeless with associated cycle storage and refuse storage and I will like to comment as follows.</p> <p>Having considered all the submitted supportive information most especially, the planning statement prepared by Sphere 25 Planning Consultancy Ltd dated November 2020 taken note of the fact that (the site is bordered by a designated ecological corridor with plan for some landscaped amenity area in the centre overlooked by all units with grassed areas/soft landscaping including a new central grassed areas and the fact that the warranty of the modular buildings will not allow the provision of PV panels), Overheating, MVHR, ASHP Technical Information Report dated 5th October 2020 taken note of the (use of community heating) and Phase I Geo-Environmental Assessment with reference DS-21906G-20-420 prepared by IDOM Merebrook Limited dated October 2020 taken note of (Sections 2.2.3, 2.2.7, 2.2.10, 2.3.2, 2.3.3, 2.13.3 and 3 (Conclusions) as well as Table 2: Summary of the key features shown on historic maps and Table 4: Preliminary Conceptual Model), please be advise that we have no objection to the development in relation to AQ and Land Contamination but the</p>	<ul style="list-style-type: none"> • Conditions have been recommended which require this.

Stakeholder	Question/Comment	Response
	<p>following planning conditions and informative are recommend should planning permission be granted.</p> <p>1. <u>Land Contamination</u> Before development commences other than for investigative work:</p> <ol style="list-style-type: none"> Using the information already acquired from the submitted Phase I Geo-Environmental Assessment with reference DS-21906G-20-420 proposed by IDOM Merebrook Limited dated October 2020, a site investigation shall be designed for the site using information obtained from the desktop study and Conceptual Model. The site investigation must be comprehensive enough to enable; a risk assessment to be undertaken, refinement of the Conceptual Model, and the development of a Method Statement detailing the remediation requirements. The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority which shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site. Where remediation of contamination on the site is required, completion of the remediation detailed in the method statement shall be carried out and; A report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority before the development is occupied. <p>Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety.</p> <p>2. <u>Unexpected Contamination</u> If, during development, contamination not previously identified is found to be present at the site then no further development (unless</p>	

Stakeholder	Question/Comment	Response
	<p>otherwise agreed in writing with the Local Planning Authority) shall be carried out until a remediation strategy detailing how this contamination will be dealt with has been submitted to and approved in writing by the Local Planning Authority. The remediation strategy shall be implemented as approved.</p> <p>Reasons: To ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels water pollution from previously unidentified contamination sources at the development site in line with paragraph 109 of the National Planning Policy Framework.</p> <p>3. <u>Construction Environmental Management Plans</u></p> <p>a. Development shall not commence (other than demolition) where applicable until a Construction Environmental Management Plan (CEMP) has been submitted to and approved in writing by the local planning authority.</p> <p>The following applies to above:</p> <p>a) The DEMP/CEMP shall include a Construction Logistics Plan (CLP) and Air Quality and Dust Management Plan (AQDMP). b) The DEMP/CEMP shall provide details of how demolition/construction works are to be undertaken respectively and shall include:</p> <p>i. A construction method statement which identifies the stages and details how works will be undertaken; ii. Details of working hours, which unless otherwise agreed with the Local Planning Authority shall be limited to 08.00 to 18.00 Monday to Friday and 08.00 to 13.00 on Saturdays; iii. Details of plant and machinery to be used during demolition/construction works; iv. Details of an Unexploded Ordnance Survey; v. Details of the waste management strategy; vi. Details of community engagement arrangements;</p>	

Stakeholder	Question/Comment	Response
	<p>vii. Details of any acoustic hoarding;</p> <p>viii. A temporary drainage strategy and performance specification to control surface water runoff and Pollution Prevention Plan (in accordance with Environment Agency guidance);</p> <p>ix. Details of external lighting; and,</p> <p>x. Details of any other standard environmental management and control measures to be implemented.</p> <p>c) The CLP will be in accordance with Transport for London's Construction Logistics Plan Guidance (July 2017) and shall provide details on:</p> <p>i. Monitoring and joint working arrangements, where appropriate;</p> <p>ii. Site access and car parking arrangements;</p> <p>iii. Delivery booking systems;</p> <p>iv. Agreed routes to/from the Plot;</p> <p>v. Timing of deliveries to and removals from the Plot (to avoid peak times, as agreed with Highways Authority, 07.00 to 9.00 and 16.00 to 18.00, where possible); and</p> <p>vi. Travel plans for staff/personnel involved in demolition/construction works to detail the measures to encourage sustainable travel to the Plot during the demolition/construction phase; and</p> <p>vii. Joint arrangements with neighbouring developers for staff parking, Lorry Parking and consolidation of facilities such as concrete batching.</p> <p>d) The AQDMP will be in accordance with the Greater London Authority SPG Dust and Emissions Control (2014) and shall include:</p> <p>i. Mitigation measures to manage and minimise demolition/construction dust emissions during works;</p> <p>ii. Details confirming the Plot has been registered at http://nrmm.london where applicable;</p> <p>iii. Evidence of Non-Road Mobile Machinery (NRMM) and plant registration shall be available on site in the event of Local Authority Inspection;</p> <p>iv. An inventory of NRMM currently on site (machinery should be regularly serviced, and service logs kept on site, which includes proof of emission limits for equipment for inspection);</p> <p>v. A Dust Risk Assessment for the works; and</p> <p>vi. Lorry Parking, in joint arrangement where appropriate.</p>	

Stakeholder	Question/Comment	Response
	<p>Reason: To safeguard residential amenity, reduce congestion and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality.”</p>	
<p>Nature Conservation / Ecology</p>	<p>Report has highlighted:</p> <ol style="list-style-type: none"> 1. There is negligible ecological interest on the site and no remaining scope for protected species to use the site in any significant way. However, bats are known to be present foraging within the Tottenham Railsides SINC and ensuring the risk of light spill onto the SINC is minimised by careful placement of any external lighting and ensuring directional shading. The SINC is though directly adjacent and young and semi-mature trees are close to the site. Care should be taken not the damage the root zones or the canopies of these trees. 2. The report has already set out measures required to ensure protection of the existing adjacent trees safeguarding the Root Protection Zones from disturbance and compaction. The external lighting design along the northern boundary needs to minimise light spill into the adjacent SINC. <p>Mitigating measures necessary to protect the adjacent SINC, namely:</p> <ul style="list-style-type: none"> • Establish root protection zones and canopy protection for boundary trees within the SINC; • Ensure light spill onto the SINC is minimised by controlling light placement and design; • Provide a robust fence for the SINC to ensure access is restricted. 	<ul style="list-style-type: none"> • Conditions have been recommended which require this.

Stakeholder	Question/Comment	Response
	<p>I am happy with the report and mitigating measures proposed and would suggest these are conditioned into the development.</p>	
<p>Waste</p>	<p>RE: Planning Application HGY/2018/2794</p> <p>LOCATION: Land to the North of Ermine Road N15</p> <p>Further to your request concerning the above planning application I have the following comments to make:</p> <p>Street-based households receiving kerbside collection services require space for the 'Standard kerbside collection full set' to be left for collection within the area of the property as close as possible to the access point to the property for collection teams. Details of the 'Standard kerbside collection full set' are given below.</p> <p>x Wheelie bins or bulk waste containers must be provided for household collections.</p> <p>Wheelie bins must be located no further than 25 metres from the point of collection.</p> <p>x Bulk waste containers must be located no further than 10 metres from the point of collection.</p> <p>x Route from waste storage points to collection point must be as straight as possible with no kerbs or steps. Gradients should be no greater than 1:20 and surfaces should be smooth and</p>	<ul style="list-style-type: none"> • A condition is attached which would require the codes, keys, transponders or any other type of access equipment to be provided to the Council and for the access to the waste storage to remain unobstructed and within 10m from Ermine Road on collection day. This would ensure suitable provision is provided.

Stakeholder	Question/Comment	Response
	<p>sound, concrete rather than flexible. Dropped kerbs should be installed as necessary.</p> <p>x If waste containers are housed, housings must be big enough to fit as many containers as are necessary to facilitate once per week collection and be high enough for lids to be open and closed where lidded containers are installed. Internal housing layouts must allow all containers to be accessed by users. Applicants can seek further advice about housings from Waste Management if required.</p> <p>x Waste container housings may need to be lit so as to be safe for residents and collectors to use and service during darkness hours.</p> <p>x All doors and pathways need to be 200mm wider than any bins that are required to pass through or over them.</p> <p>x If access through security gates/doors is required for household waste collection, codes, keys, transponders or any other type of access equipment must be provided to the council. No charges will be accepted by the council for equipment required to gain access.</p> <p>Waste collection vehicles require height clearance of at least 4.75 metres. Roads required for access by waste collection vehicles must be constructed to withstand load bearing of up to 26 tonnes.</p> <p>x Adequate waste storage arrangements must be made so that waste does not need to be placed on the public highway other than immediately before it is due to be collected. Further detailed advice can be given on this where required.</p> <p>x Other comments as follows:</p> <p>Proposal: Temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as</p>	

Stakeholder	Question/Comment	Response
	<p>accommodation for the homeless with associated cycle storage and refuse storage.</p> <p>This application will need the following</p> <p>5 x 1100L waste receptacle for refuse 3 x 1100L waste receptacle for dry recycling</p> <p>This application does not confirm the following points.</p> <ul style="list-style-type: none"> • Bulk waste containers must be located no further than 10 metres from the point of collection. • Route from waste storage points to collection point must be as straight as possible with no kerbs or steps. Gradients should be no greater than 1:20 and surfaces should be smooth and sound, concrete rather than flexible. Dropped kerbs should be installed, as necessary. • Waste collection vehicles must be able to enter and exit the development using forward motion gears. • Also check all comments provide above and below are followed. <p>The above planning application has been given a RAG traffic light status of AMBER for waste storage and collection.</p>	
<p>Lead Local Flood Authority (LLFA) - Drainage / Flooding (SuDS)</p>	<p>We've now reviewed the drawing and the FRA, for this proposed site, the LLFA, have a couple of points to make. The rainfall data in the Micro-drainage report needs to be FEH, and not FSR, as this doesn't provide the most up to date data, this will need to be updated as per the Haringey, guidance and re-submitted.</p> <p>A completed pro-forma will need to be provided in due course, and evidence that Thames Water, have given consent to connect to their network and capacity exists to receive the surface water.</p> <p>Confirmation will need to be provided with regards to the units not being placed within 8m, of the culvert as per the advice given by the EA.</p>	<ul style="list-style-type: none"> • Conditions have been recommended which require this.

Stakeholder	Question/Comment	Response
	<p>The LLFA, accept in principal the proposed controlled discharge rate of 5 l/s, and the proposed SuDS, solutions that include an attenuation system and permeable paving throughout the site. A comprehensive management maintenance schedule has been provided that will see the system function effectively for the duration of the site.</p> <p>The LLFA, are happy for the proposal to proceed at this stage.</p>	
EXTERNAL		
Environment Agency (EA)	<p>Based on our review of the submitted documents we object to this application due to its proximity (within 3 metres) to a culverted main river, the Stonebridge Brook. As submitted, it is unlikely that we would grant a flood risk activity permit for this application.</p> <p><i>Since this initial objection discussions between the applicant and the EA have been ongoing. Condition 18 has been drafted to resolve the issue but at the time of drafting the report the EA have not yet confirmed if it meets their requirements.</i></p>	<ul style="list-style-type: none"> • The applicant has employed consultants to survey the area who have only located a Thames Water surface water sewer network which is 23.9m away at its closest point. The survey information indicates that no other subterranean waterways have been located closer to the boundary. The applicant has contacted Thames Water to see if they can confirm that the sewer is the adopted watercourse – A response to this has not yet been received. • This has been provided to the EA who expect to withdraw their objection and impose a condition if necessary. • Condition 18 is recommended as a fallback. • An update on this matter will be provided as an addendum to this report.

Stakeholder	Question/Comment	Response
<p>London Fire Brigade</p>	<p>The London Fire Commissioner (the Commissioner) is the fire and rescue authority for London. The Commissioner is responsible for enforcing the Regulatory Reform (Fire Safety) Order 2005 (The Order) in London.</p> <p>The Commissioner has been consulted with regard to the above-mentioned premises and makes the following observations:</p> <p>The Commissioner is not satisfied with the proposals for fire fighting access.as access does not meet the requirements of part B5 of the building regulations.</p> <p>---</p> <p>We need to get to within 45 meters of fire hose length to the farthest room on the farthest block and from the plans we can not or they may have to provide sprinklers throughout all blocks</p>	<ul style="list-style-type: none"> • The applicant has provided a drawing which indicates that this would be possible, and this has been provided to the LFB for further comment • An update on this matter will be provided as an addendum to this report.
<p>Designing out Crime Officer (Metropolitan Police)</p>	<p>With reference the above application we have now had an opportunity to examine the details submitted and would like to offer the following comments, observations and recommendations. These are based on relevant information to this site (Please see Appendices), including my knowledge and experience as a Designing Out Crime Officer and as a Police Officer.</p> <p>It is in our professional opinion that crime prevention and community safety are material considerations because of the mixed use, complex design, layout and the sensitive location of the development. To ensure the delivery of a safer development in line with L.B. Haringey DMM4 and DMM5 (See Appendix), we have highlighted some of the main comments we have in relation to Crime Prevention (Appendices 1).</p> <p>We have met with the project Architects and Haringey Team regarding the development to discuss Crime Prevention and Secured by Design (SBD) for the overall site and are satisfied that the development aims to achieve SbD accreditation ensuring the safety and security of the occupants of the development. At this</p>	<ul style="list-style-type: none"> • A Condition is recommended that a 'Secured by Design' accreditation shall be obtained for the development prior to occupation and the features that are agreed to be necessary for security are retained for the lifetime of the development. This would ensure that the development is safe and secure and would reduce crime and the fear of crime.

Stakeholder	Question/Comment	Response
	<p>point it can be difficult to design out any issues identified. At best crime can only be mitigated against, as it does not fully reduce the opportunity of offences.</p> <p>In principle we have no objections to the site as long as proportionate measures are placed to satisfy any concerns over the overall layout of the development and its effect on the local community and the residents of the development.</p> <p>We have recommended the attaching of suitably worded conditions and an informative offering recommendations to minimize crime and the fear of crime within the development and the local community.</p> <p>The comments made can be easily mitigated if the Architects or Managing Agency were to maintain and open dialogue regarding the project prior to completion, throughout its build and by following the advice given. This can be achieved by the below Secured by Design conditions being applied (Section 2). If the Conditions are applied, we request the completion of the relevant SBD application forms at the earliest opportunity. The project has the potential to achieve a Secured by Design Accreditation if advice given is adhered to.</p> <p>Section 2 - Secured by Design Conditions and Informative: In light of the information provided, we request the following Conditions and Informative:</p> <p>Conditions: Prior to the first occupation of each building or part of a building or use, a 'Secured by Design' accreditation shall be obtained for such building or part of such building or use and thereafter all features are to be permanently retained.</p> <p>Informative: The applicant must seek the continual advice of the Metropolitan Police Service Designing Out Crime Officers (DOCOs) to achieve accreditation. The services of MPS DOCOs are available Free of</p>	

Stakeholder	Question/Comment	Response
	<p>Charge and can be contacted via docomailbox.ne@met.police.uk or 0208 217 3813.</p> <p>Section 3 - Conclusion:</p> <p>We would ask that our department's interest in this planning application is noted and that we are advised of the final Decision Notice, with attention drawn to any changes within the development and subsequent Condition that has been implemented with crime prevention, security and community safety in mind.</p> <p>Should the Planning Authority require clarification of any of the recommendations/comments given in the appendices please do not hesitate to contact us at the above office.</p> <p>Yours sincerely,</p> <p>Lee Warwick 463TP Designing Out Crime Officer Metropolitan Police Service</p>	

Stakeholder	Question/Comment	Response
<p>Transport for London</p>	<ul style="list-style-type: none"> TfL have no objections to the proposal; they require further information as set out below: <p><u>Cycle Parking</u></p> <ul style="list-style-type: none"> 1 cycle parking space should be provided for each home (Total = 37). The applicant should provide at least 2 short-stay cycle parking spaces and at least 5% (2) will need to be provided for larger and adapted cycles. <p><u>Construction Management/Logistics Plan (CMP/CLP)</u></p> <ul style="list-style-type: none"> TfL will require a construction logistics plan which highlights: <ul style="list-style-type: none"> the timeline of construction and the highway safety strategy along the access roads. And includes: the delivery times of the construction vehicles and a swept path analysis for crane vehicles carrying the necessary equipment. construction deliveries outside peak times with allocated delivery slots for site management and vehicles which are recognised in the FORS or similar. Temporary obstructions during the construction and delivery must be kept to a minimum and should not encroach on the clear space needed to provide safe passage for pedestrians or obstruct the flow of traffic on the TLRN. 	<ul style="list-style-type: none"> There is a shortfall of 1 cycle parking space which is due to insufficient room being available to place another cycle stand beneath the covered walkways. An additional stand would affect accessibility and would not be covered so would not be useable. The shortfall is considered acceptable given the specialist nature of the use which should not require maximum provision and due to it being only a deficiency of 1. A condition requiring a Construction Management/Logistics Plan (CMP/CLP) to be submitted and approved prior to the craning of the buildings on to the site is recommended.

Stakeholder	Question/Comment	Response
Thames Water	<p>Re: LAND TO THE NORTH OF , ERMINE ROAD, LONDON, N15 6DQ</p> <p>Waste Comments With regard to SURFACE WATER drainage, Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to our website. https://developers.thameswater.co.uk/Developing-a-large-site/Apply-and-pay-for-services/Wastewaterservices</p> <p>We would expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Should the Local Planning Authority be minded to approve the planning application, Thames Water would like the following informative attached to the planning permission: "A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk . Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.</p>	<ul style="list-style-type: none"> • Informatives Added

Stakeholder	Question/Comment	Response
	<p>The proposed development is located within 15 metres of our underground waste water assets and as such we would like the following informative attached to any approval granted. "The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures.https://developers.thameswater.co.uk/Developing-a-large-site/Planning-yourdevelopment/ Working-near-or-diverting-our-pipes. Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB</p> <p>There are public sewers crossing or close to your development. If you're planning significant work near our sewers, it's important that you minimize the risk of damage. We'll need to check that your development doesn't limit repair or maintenance activities, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-nearor-diverting-our-pipes.</p> <p>Thames Water would recommend that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of petrol / oil interceptors could result in oil-polluted discharges entering local watercourses.</p> <p>Thames Water would advise that with regard to WASTE WATER NETWORK and SEWAGE TREATMENT WORKS infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.</p>	

Stakeholder	Question/Comment	Response
	<p>Water Comments</p> <p>There are water mains crossing or close to your development. Thames Water do NOT permit the building over or construction within 3m of water mains. If you're planning significant works near our mains (within 3m) we'll need to check that your development doesn't reduce capacity, limit repair or maintenance activities during and after construction, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes.</p> <p>https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-nearor-diverting-our-pipes</p> <p>If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.</p> <p>On the basis of information provided, Thames Water would advise that with regard to water network and water treatment infrastructure capacity, we would not have any objection to the above planning application.</p> <p>Thames Water recommends the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.</p>	

Stakeholder	Question/Comment	Response
<p>The Greater London Archaeological Advisory Service (GLAAS)</p>	<p>Land to the North of Ermine Road London N15 Temporary planning permission for a period of 7 years to provide 38 modular residential homes for use as accommodation for the homeless with associated cycle storage and refuse storage.</p> <p>Recommend No Archaeological Requirement</p> <p>Thank you for your consultation dated 10 November 2020.</p> <p>The Greater London Archaeological Advisory Service (GLAAS) gives advice on archaeology and planning. Our advice follows the National Planning Policy Framework (NPPF) and the GLAAS Charter.</p> <p>NPPF section 16 and the Draft London Plan (2017 Policy HC1) make the conservation of archaeological interest a material planning consideration.</p> <p>Having considered the proposals with reference to information held in the Greater London Historic Environment Record and/or made available in connection with this application, I conclude that the proposal is unlikely to have a significant effect on heritage assets of archaeological interest.</p> <p>In view of the limited groundworks connected with the modular scheme, I do not advise archaeological measures in this case. No further assessment or conditions are therefore necessary.</p> <p>This response relates solely to archaeological considerations. If necessary, Historic England's Development Advice Team should be consulted separately regarding statutory matters.</p>	<ul style="list-style-type: none"> • Noted

Stakeholder	Question/Comment	Response
<p>Natural England</p>	<p>Thank you for getting in touch about the above consultation, please find Natural England's response below.</p> <p>Natural England has no comment on this application with regards to designated sites. However, Natural England notes the importance of Sustainable Urban Drainage Systems (SuDS) and that there may be opportunities for green infrastructure and biodiversity enhancements on the development site.</p> <p>This application may provide opportunities to incorporate features into the design which are beneficial to wildlife, such as the incorporation of roosting opportunities for bats or the installation of bird nest boxes. The authority should consider securing measures to enhance the biodiversity of the site from the applicant, if it is minded to grant permission for this application. This is in accordance with Paragraph 118 of the NPPF.</p> <p>Natural England is supportive of the inclusion of living roofs where they are appropriate to developments. Research indicates that the benefits of green roofs include reducing run-off and thereby the risk of surface water flooding; reducing the requirement for heating and air-conditioning; and providing habitat for wildlife. We would advise your council that some living roofs, such as sedum matting, can have limited biodiversity value in terms of the range of species that grow on them and habitats they provide. Natural England would encourage you to consider the use of bespoke solutions based on the needs of the wildlife specific to the site and adjacent area.</p> <p>We also highlight paragraphs 163 and 165 of the NPPF which relate to sustainable drainage systems. We encourage use of Sustainable urban Drainage Systems (SuDS) on site, and it may be possible for SuDS measures to provide multifunctional benefits such as biodiversity improvements.</p> <p>Natural England has not assessed this application for impacts on protected species. Natural England has published Standing</p>	<ul style="list-style-type: none"> • Noted – Ecology conditions recommended

Stakeholder	Question/Comment	Response
	<p>Advice which you can use to assess impacts on protected species or you may wish to consult your own ecology services for advice.</p> <p>It is for the local planning authority to determine whether or not this application is consistent with national and local policies on the natural environment. Other bodies and individuals may be able to provide information and advice on the environmental value of this site and the impacts of the proposal to assist the decision making process. We advise LPAs to obtain specialist ecological or other environmental advice when determining the environmental impacts of development.</p> <p>Your authority has a duty to have regard to conserving biodiversity as part of your decision making. Conserving biodiversity can also include restoration or enhancement to a population or habitat. Further information is available here.</p> <p>Kind regards, Isabella Jack</p> <p>Isabella Jack – Sustainable Development Adviser Thames Solent Team Natural England</p>	

Stakeholder	Question/Comment	Response
<p><i>[Rail]</i></p> <p>Network Rail</p> <p>London Overground</p> <p>Crossrail 2 (Safeguarding Direction)</p>	<ul style="list-style-type: none"> • Network Rail have no objections to the proposals. • London Overground Infrastructure had no comments on the proposals. • The site is an Area of Surface Interest in the 2015 Crossrail 2 Safeguarding Directions. In the event of a decision to progress the Crossrail 2 project the land may be subject to compulsory purchase to provide a worksite for the future delivery of the Crossrail 2 scheme. 	<ul style="list-style-type: none"> • Noted • Noted • London Plan (2016) Policy 6.4 requires the Mayor will work with strategic partners to improve the public transport system in London by developing Crossrail 2. Publication London Plan Policy T3 under para. D also requires development decisions to give priority to securing and supporting the delivery of Crossrail 2. As the site is safeguarded – only temporary solutions would be realistic on the site given the capital expenditure for any longer term installations. This limits the opportunity for optimising the site and delivering housing. The proposal responds to these constraints with a modular build that provides specialist housing for a specific need.

Appendix 4 Representations from Neighbours

- 206 Neighbouring properties were notified of the application and 59 letters of objection have been received. The main objections are listed below with Officer responses listed in the right hand column.
- 1 letter of support has been received which stated the following:
 - *Despite the propaganda from neighbours which includes a note through the door urging all local residents to reject this application, I am pleased to support it. Homeless people need somewhere to live. As the Lord Jesus Himself said 'it is more blessed to give than receive'.*
- A petition against the proposal was also received which indicates that residents from 54 local households wish to object to the proposals. It is understood that some of the signatories wish to remain anonymous, so their names and addresses were not displayed on the public register. Anonymous objections are generally considered to have less weight than those where names and addresses are provided.

Potential for an increase in crime (antisocial behaviour)	
<u>Objection</u>	<u>Response</u>
<ul style="list-style-type: none"> • Residents who live near this site are concerned that there could be an increase in anti-social behaviour. • There appears to be no specific plan in place to ensure this does not happen other than the existing structures which are overstretched. 	<ul style="list-style-type: none"> • All occupants would be referred to the service using a robust referral process, led by the Haringey Street Outreach Team and the Council's Homelessness Pathway Team. • There would be no direct access to the service without a prior referral and assessment of suitability. This ensures that vulnerabilities and support needs are explored with each person before they move in. This approach ensures the safety of the occupants as well as that of others. • The site design is intended to reduce opportunities for anti-social behaviour by creating a well-lit and visible environment with appropriate but not invasive CCTV. There would always be a member of staff on site. This would be a support worker in the daytime and a concierge

	<p>at night. The staff office is positioned with good visibility across the site which would enable the management of visitors to the site at all times.</p> <ul style="list-style-type: none"> • The scheme has also been reviewed by a Designing out Crime Officer at the Metropolitan Police who has stated that they cannot see any reason why the development cannot achieve accreditation according to Secured by Design (SBD) guidelines. • Condition 17 has also been recommended which requires SBD accreditation to be achieved prior to occupation.
<ul style="list-style-type: none"> • There is little confidence that the site will be managed well in terms of general maintenance - landscaping, refuse collection, car parking but also managing the day to day needs of vulnerable people who live in these units. • Very vulnerable people will be housed here yet there seems to be very little in the way of facilities. 	<ul style="list-style-type: none"> • The homes would be steppingstones to help people transition from rough sleeping. The proposal would give them time to adjust to living in their own place and to develop their independent living skills. Following this they will move on to their own permanent homes. • The service would operate year round and would be staffed 24 hours per day by a combination of specialist support workers and night concierge. The support workers would help to manage the day to day needs of residents whilst also be present to report any issues that may occur in relation to general maintenance. • The homes were designed by Hill architects who worked closely with several leading homeless charities including The Connection at St Martins in the Field on Trafalgar Square. Hill undertook several stakeholder and user group forums to discuss the design in depth. • The units are thoughtfully designed through consultation with individuals who have slept rough or have had experiences of homelessness. They would feel homely and the space would be well utilised.

Noise & disturbance	
<u>Objection</u>	<u>Response</u>
<ul style="list-style-type: none"> There would be undue noise from the new residents. 	<ul style="list-style-type: none"> There would always be a member of staff present who could deal with any issues in this regard.
<ul style="list-style-type: none"> The build will undoubtedly cause disruption and inconvenience to residents if it commences. 	<ul style="list-style-type: none"> The construction method would be one of modular build which is significantly quicker than traditional construction and takes a few days. Condition 6 requires a Construction Management Plan/Logistics Plan to be submitted which would ensure that disruption resulting from construction is minimised. This will safeguard residential amenity, reduce congestion, and mitigate obstruction to the flow of traffic, protect air quality and the amenity of the locality.
Impact on neighbouring amenity & parking	
<u>Objection</u>	<u>Response</u>
<ul style="list-style-type: none"> I am concerned for the privacy of my property - said units are facing and directly overlook my house and bedroom windows. 	<p>The orientation of the buildings would minimise overlooking. Block A would be the closest block to the existing properties to the south of Ermine Road.</p> <ul style="list-style-type: none"> Block A would not directly face the properties to the south of Ermine Road and given the distances between them, the level of overlooking would be commensurate with the character of the locality. The proposed units also have relatively modest windows and given the siting and orientation; the privacy of existing residents would not be adversely affected to a material degree by the proposal.

<ul style="list-style-type: none"> This area is too small for more properties and with the parking situation at present 	<ul style="list-style-type: none"> The site is close to alternative and accessible means of transport, it has a high PTAL, and a Controlled Parking Zone (CPZ) exists near to the site. One parking space would be provided for disabled people in line with Publication London Plan standards. It therefore provides a policy compliant level of parking. It is highly unlikely given the above and the length of tenancy that residents would own a car. In any event, Condition 4 is recommended which would ensure that the development would not be occupied other than through a lease/license that ensures that residents are obligated not to apply for a parking permit for any CPZ.
<ul style="list-style-type: none"> There are already problems with waste management and the proposal would make this worse. 	<ul style="list-style-type: none"> The proposal provides sufficient waste storage through the provision of a refuse store which holds 5 x 1100L waste receptacles for refuse & 3 x 1100L waste receptacles for dry recycling. The plans indicate that the waste containers would be located no further than 10 metres from the point of collection (which would be on Ermine Road). The site would also have staff on site 24 hours a day who can respond to any issues relating to missed waste collections or problems related to waste on the site.
<ul style="list-style-type: none"> There will be issues with light pollution and overshadowing 	<ul style="list-style-type: none"> There would be lighting in the scheme, that would be commensurate with what is in the locality and, as such, would not result in a material change that would result in harm to the amenity of nearby residents.

	<ul style="list-style-type: none"> Given the low height of the buildings (2 storeys) and the distance to neighbours there would be no overshadowing of adjacent properties.
Design	
<u>Objection</u>	<u>Response</u>
<ul style="list-style-type: none"> The units are not in coordination with the properties on the estate and would stand out and be an eyesore. Modular Homes of a bright Orange appearance how do they fit into the character and appearance of the Estate. 	<ul style="list-style-type: none"> The Council's design officer has been consulted and notes that the development provides an exemplary example of modular building design, in a form that is of gentle density and appearance – which is compatible with the varied but mostly residential context. The 2 storey scale of development would be within and below the prevailing height and bulk of the surroundings. The rhythm of windows and doors, panels, and colours, would provide an attractive elevational / fenestration pattern of a comparable scale and proportion to the ordinary domestic surroundings, whilst being clearly of contemporary appearance expressing its progressive modern construction.
Scale/no. of units & the size of the units	
<u>Objection</u>	<u>Response</u>
<ul style="list-style-type: none"> There are too many units on a small site. 	<ul style="list-style-type: none"> In line with Publication London Plan Policy D3 Optimising site capacity through the design-led approach – the development makes the best use of land by following a design led approach that optimises the capacity of the site.

	<ul style="list-style-type: none"> • This is specialist housing that meets the specific needs of the prospective residents. • The maximum number of homes are being delivered so that housing and support can be provided for as many people as possible. • The site layout and design has been carefully considered so that it meets planning requirements as well as the specific needs of prospective residents.
<ul style="list-style-type: none"> • It would be inhuman to cram up 38 modular houses in a small space, for people, who are already facing traumatic challenges in life. 	<ul style="list-style-type: none"> • The proposed homes would be 24sqm which is smaller than the 37sqm recommended for a 1-bed flat with a shower room in the Technical housing standards produced by the Department for Communities and Local Government. They have been designed as specialist housing for temporary accommodation to support people who have been street homeless at a low cost. • The homes have been designed to be large enough to enable a good quality home for people that have experienced homelessness, while ensuring residents can manage the home and are not vulnerable to exploitation through 'cuckooing' (a practice where people take over a person's home and use the property to facilitate exploitation).

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Planning Sub Committee

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS

Reference No: HGY/2020/3036

Ward: Woodside

Address: Rear of 132 Station Road N22 7SX

Proposal: Construction of 6 dwellings set in landscaped area and creation of 'community wildlife garden', following the demolition of existing structures.

Applicant: Arden Property Limited

Ownership: Private

Case Officer Contact: Laurence Ackrill

Site Visit Date: 23/12/2020

1.1 This application has been brought before the committee following councillor referral (Cllr. Peter Mitchell).

1.2 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of backland development is considered acceptable, following a detailed assessment of the scheme overall.
- The proposed development would be of a high-quality design and would enhance the character and appearance of the Conservation Area overcoming the previous reason for refusal at appeal.
- The impact of the development upon the residential amenity of neighbouring occupiers is acceptable.
- The proposal would offer a high-quality form of accommodation for future occupants.
- There would be no significant impact on parking or the transport/highways network.
- The proposal would not have a significant impact on biodiversity, would not result in the loss of any designated nature conservation or public open space.
- The excavations to create the proposed basements would not cause significant harm to adjoining properties or increase flood risk subject to detailed conditions.
- Site access arrangements would be sufficient for the purposes of carrying out the development.
- Satisfactory waste collection arrangements can be secured by way of condition.

2. RECOMMENDATION

- 2.1 That the Committee resolve to GRANT planning permission and that the Head of Development Management or Assistant Director for Planning, Building Standards and Sustainability is authorised to issue the planning permission and impose conditions and informatives subject to the signing of a section 106 Legal Agreement providing for the obligation set out in the Heads of Terms below.
- 2.2 That delegated authority be granted to the Head of Development Management or the Assistant Director Planning, Standards and Sustainability to make any alterations, additions or deletions to the recommended heads of terms and/or recommended conditions as set out in this report and to further delegate this power provided this authority shall be exercised in consultation with the Chairman (or in their absence the Vice-Chairman) of the Sub-Committee.
- 2.3 That the section 106 legal agreement referred to in resolution (2.1) above is to be completed no later than 08/04/2021 or within such extended time as the Head of Development Management or the Assistant Director Planning shall in her/his sole discretion allow; and
- 2.4 That, following completion of the agreement(s) referred to in resolution (2.1) within the time period provided for in resolution (2.2) above, planning permission be granted in accordance with the Planning Application subject to the attachment of the conditions.

Conditions (the full text of recommended conditions is contained in Appendix 1 of this report)

- 1) Development begun no later than three years from date of decision
- 2) In accordance with approved plans
- 3) Materials submitted for approval
- 4) Written scheme of investigation
- 5) Details of lighting
- 6) Cycle storage
- 7) Refuse storage
- 8) Hard and soft landscaping
- 9) Construction management plan
- 10) AQDMP
- 11) Considerate constructor scheme
- 12) Desktop study contamination
- 13) Contamination remediation
- 14) Tree protection fencing
- 15) Green / Meadow roof details
- 16) Restrict vegetated roof as amenity area
- 17) Details of enclosures
- 18) Restrict PD rights

- 19) Qualified chartered engineer
- 20) Drainage strategy
- 21) Overheating
- 22) Energy Strategy
- 23) Delivery and service plan

Section 106 Heads of Terms:

- 1) Car free
- 2) Car club
- 3) Community use agreement

Presumption in Favour of Sustainable Development

- 2.5 In the event that members choose to make a decision contrary to the officer recommendation (that the proposed development accords with the development plan overall), it will be necessary to consider the presumption in favour of sustainable development in the NPPF. This is because the Council's delivery of housing over the last three years is substantially below its housing target and so paragraph 11(d) of the NPPF is engaged by virtue of footnote 7 of the NPPF. Members must state their reasons including why it is considered that the presumption is not engaged.
- 2.6 That, in the absence of the agreement referred to in resolution (2.1) above being completed within the time period provided for in resolution (2.2) above, the planning permission be refused for the following reasons:
- 1. *The development, in the absence of a legal agreement does not include a formal undertaking to secure a contribution to allow the modification of the existing traffic order to exempt future occupants of the proposal from purchasing parking permits and alterations to the public highway, arising as a result of the development. As such, the proposal is contrary to Policy 6.13 of the London Plan 2016, SP7 of the Local Plan 2017 and Policy DM32 of the Development Management Development Plan Document 2017.*
- 2.7 In the event that the Planning Application is refused for the reasons set out in resolution (2.6) above, the Head of Development Management (in consultation with the Chair of Planning sub-committee) is hereby authorised to approve any further application for planning permission which duplicates the Planning Application provided that:
- (i) There has not been any material change in circumstances in the relevant planning considerations, and
 - (ii) The further application for planning permission is submitted to and approved by the Assistant Director within a period of not more than 12 months from the date of the said refusal, and

(iii) The relevant parties shall have previously entered into the agreement contemplated in resolution (1) above to secure the obligations specified therein.

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4. CONSULTATION RESPONSE
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9. PLANNING CONDITIONS & INFORMATIVES

APPENDICES:

- | | |
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3.0 PROPOSED DEVELOPMENT AND LOCATION DETAILS

3.1 Proposed development

- 3.1.0 This is an application for the demolition of existing structures and construction of 6 dwellings (1 x two storey dwelling with basement & 5 x single storey dwellings with basement) set in a landscaped area, and the creation of a community wildlife garden with public access.

3.2 Site and Surroundings

- 3.2.1. The application site relates to a plot of land which previously served as a private garden belonging to 132 Station Road. Mapping and site visit evidence suggest it has not been used as a residential garden for many years. The site is located to the east of the New River, and to the rear of gardens serving terraced houses along Station Road to the south, Park Avenue to the north west and Barrett Avenue to the north. The site is accessed via a passageway which opens on to Station Road currently serving 140 Station Road which adjoins the site to the west. It includes a number of single storey, somewhat dilapidated structures / sheds.
- 3.2.2. The site is within the Wood Green Common conservation area. Whilst there are no listed buildings within the site, the Grade II listed New River tunnel entrance is located on land which adjoins the site to the west. The New River itself is locally listed.

3.3 Relevant Planning and Enforcement history

- 3.3.0 HGY/2017/2182 - Land at the rear of 132 Station Road London N22 7SX London - Demolition of existing structures and redevelopment of the site to provide 3 no. two storey family sized dwellings (with basement floors) and associated refuse shelters, cycle parking and additional landscaping. – Refused - 22/01/2018. Appeal reference APP/Y5420/W/18/3196614 - Appeal dismissed - 29/06/2018.
- 3.4.0 HGY/2020/1841- Construction of 6 dwellings set in landscaped area and creation of community wildlife garden, following the demolition of existing structures. Withdrawn 12/10/2020.

4. CONSULTATION RESPONSE

The following were consulted regarding the application:

- 1) LBH Transportation Officer
- 2) LBH Conservation Officer
- 3) LBH Design Officer
- 4) LBH Drainage Officer

- 5) LBH Arb Officer
- 6) LBH Carbon Management
- 7) LBH Building Control
- 8) Avenue Gardens Residents Association

5. LOCAL REPRESENTATIONS

5.1 The following were consulted:

74 Neighbouring properties
1 Residents' Association
1 site notice erected close to the site
Press notice published

5.2 The number of representations received from neighbours, local groups etc in response to notification and publicity of the application were as follows:

No of individual responses: 60
Objecting: 59
Supporting: 1
Others: 0

5.3 The following local groups/societies made representations:

- None

5.4 The following Councillor made representations:

- Cllr Peter Mitchell – objects on the following grounds (summary):
Impact on the conservation area has not overcome planning inspector comments; Creation of new garden area does not overcome the planning inspector comments; Impact on the character and appearance of the conservation area and overdevelopment of the site; Living conditions for future residents; Impact on parking; Nature conservation impact; Impact on neighbours.

5.5 The issues raised in third party representations that are material to the determination of the application are set out in Appendix 2 and summarised as follows:

- Housing needs are already being met
- Noise and disturbance
- Increase in traffic
- Out of character with the open space / conservation area
- Light pollution

- Loss of biodiversity / wildlife / protected species
- Safety concerns during construction
- Materials at odds with conservation area
- Plumbing and drainage issues
- Overlooking and loss of privacy
- Re-development of existing building on site overbearing
- Loss of employment
- Overdevelopment of the site
- Impact from the basement
- Security issues
- Archaeological impacts

MATERIAL PLANNING CONSIDERATIONS

The main planning issues raised by the proposed development are:

1. Planning history context
2. Principle of the development
3. The impact on the amenity of adjoining occupiers
4. The impact of the proposed development on the character and appearance of the conservation area
5. Living conditions for future occupants
6. Parking and highway safety
7. Trees and ecology; and
8. Basement development

6.1 Planning history context

6.1.0 The application follows a previous refused application under reference HGY/2017/2182 determined in 2018 by the planning sub-committee and was subsequently dismissed at appeal under reference APP/Y5420/W/18/3196614. The proposal as part of that application involved the demolition of existing single storey structures on the site and the construction of 3 no. two storey family sized dwellings, over ground floor and basement levels.

6.1.1 The reasons for refusal as part of the refused application included the following:

1. *The proposed development, by reason of the quantum of development and domestication of the land, would be harmful to the character and appearance of the area and represent an overdevelopment of the site.*
2. *The general access arrangements proposed to service the development would not result in a high quality residential environment.*

- 6.1.2 The Planning Inspector as part of the appeal decision upheld the 1st reason for refusal in relation to the impact the development would have on the character and appearance of the conservation area. However, they considered that the proposed development would provide adequate living conditions for future residents in terms of access arrangements.
- 6.1.3 The proposed development has been altered significantly since the submission of the previous application. The site itself is larger, encompassing two small adjacent sites since the previous appeal. The number of dwellings proposed has increased from 3 to 6, including the re-development of the existing two 'Coach House' building (not within the site area at the time of the appeal). 5 of the new dwellings would comprise of an undulating 'meadow roof' with a substrate level of soil. The site also now incorporates the entirety of the land to rear of properties along both Station Road and Barratt Avenue, and proposes a publicly accessible community garden area to the east from Barratt Avenue.

6.2 Principle of the development

Delivering new housing

- 6.2.0 Government policy as set out in the NPPF 2019 requires Local Planning Authorities to significantly boost the supply of housing (para. 59). Paragraph 68 supports approval on small sites and outlines that such sites can make an important contribution to meeting the housing requirement of an area, and often can be built out relatively quickly.
- 6.2.1 The principle of additional housing is supported by the London Plan (2016) Policies 3.3 'Increasing Housing Supply' and 3.4 'Optimising Housing Potential'. Local Plan Policy SP1 sets out the strategic vision to provide up to 20,410 new homes by 2026, which aligns with the aspirations of Policy SP2 which has a current target of providing 1,502 new homes a year in Haringey between the period 2015 to 2026, in line with the London Plan (MALP) 2016. The creation of an additional housing unit here is supported by Local Plan Policies SP1, SP2 and London Plan Policy 3.3.
- 6.2.2 Third party objectors object on ground of housing needs already being met. While Haringey is delivering housing more is required to meet targets. It is also noted that these targets are minima; there is no maximum set.
- 6.2.3 The intend to publish London Plan policy on small sites (H2) is afforded weight in the determination of this application. The plan, expected to be adopted in 2021, has been 'examined in public' and as such carries weight in the decision-making process Policy H2 set out a presumption in favour of small sites and seeks to promote infill development on vacant or underused sites within PTALs 3-6 and within 800m of a Tube or rail station. The site is located within 800m of both tube (Wood Green) and rail stations (Alexandra Palace), the site is also within close

proximity to the Wood Green district town centre and within a PTAL 5 area which is considered very good. A wide variety of 24-hour bus services are accessible from Wood Green within a 10-minute walk of the site, with W3 bus stops being located within a minutes' walk of the application site along Station Road, which also provides a 24-hour service.

Housing delivery test

- 6.2.4 The 2020 Housing Delivery Test (HDT) results were published on 19 January 2021 and as a result the LPA is now subject to the 'presumption in favour of sustainable development' and paragraph 11d of the NPPF is relevant. The Council's delivery of housing over the last three years is substantially below its housing target and so paragraph 11d) of the NPPF is engaged by virtue of footnote 7 of the NPPF. Nevertheless, the proposed development has been found to be in accordance with development plan policies and therefore consideration of para 11(d) is not required in this instance (but would be, if the application was to be refused).

Infill /backland development

- 6.2.5 Part A of Policy DM7 of the Council's adopted 'Development Management DPD' 2017 states that there will be a presumption against the loss of garden land unless it represents comprehensive redevelopment of a number of whole land plots.
- 6.2.6 The Council's Urban Characterisation Study (2015) identifies various urban typologies where the built form relies on more or less regular street forms, building facades, and garden areas where developments on back gardens are likely to have a negative impact on the character of the area and the integrity of the street scene. Back gardens are also an important ecological resource and play a significant role in drainage and flood mitigation. The Council therefore considers back garden development to be generally inappropriate and at odds with the spatial strategy of the Borough, which seeks to focus development in growth areas well served by transport and local amenities. There are in some cases exceptions to this, for example, where sites can be assembled to bring forward comprehensive development and can be designed to provide an appropriate layout consistent with the surrounding character and amenity.
- 6.2.7 Part B of Policy DM7 highlights 7 sub-points amongst which any proposal must relate sensitively to the surrounding area as well as the established street scene, provide a site specific and creative response to the built and natural features of the area and safeguard privacy, and amenity.
- 6.2.8 Despite the plot having been historically associated as a private garden, it is an anomaly in that it does not conform with the layout of development in the area which is characterised by terraced houses on rectangular plots with regularly sized garden areas to the front and rear. The proposed development would not

result in the loss of private garden space to any of the existing properties along Station Road or Barrett Avenue.

- 6.2.9 On balance, the proposed development is considered sensitive in scale and footprint to the surrounding built form and pattern of development. The scheme has been carefully designed and is considered an architecturally ambitious approach to developing with landscape and ecology in mind.
- 6.2.10 Overall the principle of development is considered acceptable per se, subject to satisfying other policy objectives, as discussed later in this report.

Provision of open space

- 6.2.11 Policy DM20 of the Councils Development Management DPD states that development that protects and enhances Haringey's open spaces will be supported. Whilst the current site does not fall within an area of designated open space, the proposal involves the creation of a community garden area that would be accessible to members of the public. Planning policy at all levels recognises the importance of open space to supporting sustainable development. High quality open space can make an important contribution to the health and well-being of communities.
- 6.2.12 The provision of public open space (approximately 200m²) would provide a public benefit given that the existing site is not open to the public, and would weigh in favour of the development. A section 106 agreement would be required to ensure that it is the responsibility of the developers / occupiers of the site to maintain that area and to ensure public access is retained.

Loss of employment

- 6.2.13 Whilst the existing 'Coach House' on the site may have been used for employment purposes (it was last used as a music recording studio), the site is not located within a designated area for employment. In addition, the locality of the site is characterised by residential dwellings and the studio is very modest. As such, the use of the site for residential purposes would be more appropriate than that of any commercial use. Given the relatively small scale nature of the building in question, the level of employment loss would be insignificant and would be outweighed by the provision of the creation of additional housing delivery on the site in this case.

Site access and Security

- 6.2.14 Development Management DPD 2017 policy DM2 'Accessible and Safe Environments' states that all proposals should ensure that new developments can be used safely, easily and with dignity by all; are designed so that the layout improves people's access to social and community infrastructure, including local

shops and public transport; protect, improve and create, where appropriate, safe and accessible pedestrian and cycling routes and should not impede pedestrian and cycling permeability; and have regard to the principles set out in 'Secured by Design'.

- 6.2.15 The creation of a residential use in this location would have minor material benefits to the security of the area including increasing activity in what is currently a largely disused backland plot, increased passive surveillance by future residents and the creation of a greater sense of ownership. As such the proposal would be in line with the principles of 'Secured by Design' and therefore would accord with policies DM2 and DM7.

6.3 Impact on the amenity of adjoining occupiers

- 6.3.0 The London Plan (2016) Policy 7.6 Architecture states that development must not cause unacceptable harm to the amenity of surrounding land and buildings. DM Policy (2017) DM1 'Delivering High Quality Design' states that development proposals must ensure a high standard of privacy and amenity for the development's users and neighbours. The Council will support proposals that provide appropriate sunlight, daylight and open aspects (including private amenity space where required) to all parts of the development and adjacent buildings and land provide an appropriate amount of privacy to their residents and neighbouring properties to avoid overlooking and loss of privacy detrimental to the amenity of neighbouring residents and the residents of the development and address issues of vibration, noise, fumes, odour, light pollution and microclimatic conditions likely to arise from the use and activities of the development.
- 6.3.1 The proposed 5 two storey dwellings located at ground and lower ground floor level would be sited with their rear elevations facing toward the rear of properties along Barratt Avenue. The height of these dwellings would have a maximum of approximately 3.5m in height above the existing ground level, and would comprise of an undulating roof that would decrease in height toward the rear gardens of those properties. This would appropriately mitigate against the visual impact upon those neighbouring occupiers in terms of appearing overbearing, resulting in a perceived sense of enclosure or loss of daylight. These dwellings would have small lightwell features that would be located at ground level, but would not provide any windows in the elevation facing Barratt Avenue as to protect privacy to those neighbouring occupiers sufficiently.
- 6.3.2 The main openings for these dwellings would be to the south, facing toward the rear of properties along Station Road. However, these windows would be located either at ground floor level or lower ground floor level. Some views may be had toward upper floor windows of properties along Station Road. However, these views would be sufficiently oblique as not to result in a significant loss of privacy. There would also be substantial soft landscaping measures provided to that

boundary that would aid in sufficiently reducing the perception of being overlooked and would be secured by way of condition.

- 6.3.3 The proposed two storey dwelling with ground and first floors would replace an existing two storey building in that location. Whilst the replacement building would be wider than that of the existing, it would be lesser in height and would also comprise of an undulating roof form that would reduce in height toward the rear where the closest neighbouring boundaries are along Barratt Avenue. As such, this element of the development would not appear significantly more overbearing or result in a loss of outlook or daylight to neighbouring occupiers over and above the existing site circumstances. There would be no upper floor windows facing directly toward neighbouring properties, with the windows facing towards either the middle of the application site or toward the entrance to the site from Station Road, similar to the positioning of upper floor windows within the existing two storey building on the site.
- 6.3.4 In terms of light and noise disturbance, the proposal would involve the provision of residential dwellings within an existing residential area. As such, it is not considered that the proposed development would result in excessive levels of noise disturbance over and above the use of existing residential dwellings within the locality of the site. Whilst it is noted that the existing site is heavily overgrown with soft landscaping, soft-landscaping measures are proposed as part of the development to help mitigate against any levels of light or noise created from the development. It is accepted that elements of lighting from the development may become apparent in a location where non currently exists. However, this would not constitute harm to the amenity of neighbouring occupiers in terms of disturbance. A condition relating to lighting details can be secured by way of condition.
- 6.3.5 Whilst the dwellings would be somewhat visible from upper floor windows of neighbouring properties, this would not constitute harm to the amenity of neighbouring occupiers. Overall, there would be no unacceptable harm to the living conditions of neighbouring residents. As such, the scheme is considered to be in accordance with policies outlined above.

6.4 Design

- 6.4.0 DM Policy (2015) DM1 'Delivering High Quality Design' states that development proposals should relate positively to their locality, having regard to, building heights, form, scale & massing prevailing around the site, urban grain, sense of enclosure and, where appropriate, following existing building lines, rhythm of any neighbouring or local regular plot and building widths, active, lively frontages to the public realm, and distinctive local architectural styles, detailing and materials. Local Plan (2017) Policy SP11 states that all new development should enhance and enrich Haringey's built environment and create places and buildings that are high quality, attractive, sustainable, safe and easy to use. Development shall be

of the highest standard of design that respects its local context and character and historic significance, to contribute to the creation and enhancement of Haringey's sense of place and identity which is supported by London Plan (2016) Policies 7.4 and 7.6.

- 6.4.1 Good quality contemporary buildings are generally seen as an appropriate architectural response for new buildings rather than a mock or pastiche of an earlier architectural style. In this case, the proposed building would not compete or undermine any of the traditional architectural styles found within the locality.
- 6.4.2 Five of the proposed dwellings would be read as single storey buildings (with additional habitable space provided at basement level). As such the development would appear subservient to the adjacent two storey terraced housing in accordance with policy DM7(f). As per the assessment of the previous application at the site, the lack of a street frontage is noted. However, it is recognised that the site does not allow for this to be achieved.
- 6.4.3 The proposed 'Coach House' dwelling would replace an existing two storey structure in a similar location. Given the similarities in the scale of the built form in that location, it is considered that this element of the scheme would have a similar impact to that of the existing building in terms of its visual prominence.
- 6.4.4 An undulating substrate 'meadow roof' is also proposed to the roof tops of the terrace of five dwellings, which would provide a natural appearance to the dwellings and would soften and integrate the buildings into the surrounding context (further details of which can be secured by way of a planning condition).
- 6.4.5 The proposed hard landscaping materials are permeable, robust and durable elements that would weather well with low maintenance requirements, such as Corten steel for raised planters, gabion walls, and the 'Grasscrete' provides a permeable, and visually fitting surfacing for vehicular access. The overall palette of materials, including the use of white brick, is high quality and well balanced, integrating well with the natural environment.
- 6.4.6 The use of high quality materials is an important part of the justification for the proposed development being considered an acceptable scheme here. As such, notwithstanding the submitted information, a condition is being attached to ensure that physical samples be submitted for further consideration. Subject to the conditions mentioned above it is considered that the external appearance and design of the building together with the proposed landscaping along the perimeters of the site will achieve a scheme of high quality design sensitive to its surroundings.
- 6.4.7 Overall, the concept is considered to be respectful of the landscaped character of the site as it is and is subordinate to the surrounding buildings. There is no in principle objection to the proposed design and this is considered the way forward

in providing a natural / seamless appearance to best reflect the context of the existing site and neighbouring residential use.

6.5 Character and appearance of the conservation area

- 6.5.0 London Plan Policy 7.8 requires that development affecting heritage assets and their settings to conserve their significance by being sympathetic to their form, scale and architectural detail. Haringey Local Plan Policy SP12 requires the conservation of the historic significance of Haringey's heritage assets. Policy DM9 of the Development Management, Development Plan Document (2015) states that proposals for alterations and extensions to existing buildings in Conservation Areas should complement the architectural style, scale, proportions, materials and details of the host building and should not appear overbearing or intrusive.
- 6.5.1 The development site lies within Wood Green Common Conservation Area, in close proximity to the New River, to Avenue Gardens and to the Common and is significantly constrained by the residential terraces which were erected between the end of the 19th century and early 20th century respectively along Barratt avenue and Station Road. Since then. The site has been framed to the north and south by the back gardens of the terraces and seems to have been independently used. It has an almost triangular shape with an east-west orientation and is accessed from Station Road via an entrance route which runs along the west flank of the end of terrace at No 138. The route leads to the back of the terrace, where there is a two storey brick building probably built at the same time as the terrace along Station road, as historic 1914 OS maps seem to suggest.
- 6.5.2 The site is currently in poor condition and cluttered with dense, overgrown vegetation and a number of run-down sheds which detract from the character and quality of the Conservation Area.
- 6.5.3 It is noted that as part of the appeal decision relating to the previously refused application at the site, that the Planning Inspector considered that '*the site also provides an important contrast to the bustle of the surrounding streets in a busy urban area*'. '*It is quiet and tranquil, providing a degree of spaciousness within the tight grain of the terraces*' with this degree of spaciousness contributing positively to the significance of the Conservation Area.
- 6.5.4 The proposal has been developed in consultation with both conservation and urban design officers and originates from a comprehensive design exploration based on clear understanding of the green and visually open character of the site as well as from full appreciation of its spatial and visual relationship with the back gardens of the surrounding residential terraces.
- 6.5.5 The proposed development is purposely founded on a lower level than the existing residential terraces so to respect and retain the primacy of the surrounding

terraces and is integrated in its landscape, so to not overwhelm the existing rear gardens and so to not detract from the views from the rear elevations of the terraces. The scheme aims to provide the highest level of integration possible with the natural and built landscape of the Conservation Area and is designed to retain the green, open and self-contained character of the site as well as improving its landscaped and built quality and the views from the surrounding houses into the site. This is considered to respond to the views of the Planning Inspector on appeal.

- 6.5.6 The curved forms of the proposed development, the undulate building line and the green roofs aim to mitigate the impact of new development on the surrounding private gardens and create a coherent site experience together with the proposed community garden on site.
- 6.5.7 The existing 'coach house', sits in a secluded location far from the street-front and is constrained in the north-west corner of the development site. Besides being an old building it is not identified or designated as a heritage asset or positive contributor to the Conservation Area, which would require at least a degree of architectural and historic interest or townscape merit, and there is no presumption for said structure to be retained.
- 6.5.8 Given the above, the proposed development is considered to be of appropriate scale, massing and architectural quality and would preserve the character and appearance of the conservation area without causing harm, arguably improving its quality and is therefore considered to comply with policy SP12 and design policies SP11 and DM1, subject to conditions in relation to materials and design specifications.
- 6.5.9 As a result of the incorporation of all of the backland area in this street-block (as opposed to the smaller area previously considered at appeal), the bespoke design-response proposed and the addition of new open space, it is considered that the concerns raised on appeal have been addressed.

6.6 Quality of Residential Accommodation

- 6.6.0 London Plan (2016) policy 3.5 requires the design of all new housing developments to enhance the quality of local places and for the dwellings in particular to be of sufficient size and quality. Local Plan (2017) Strategic Policy SP2 and Policy DM12 of the Development Management DPD 2017 reinforce this approach. The Mayor's Housing SPG sets out the space standards for new residential developments to ensure an acceptable level of living accommodation is offered
- 6.6.1 In assessing the proposal against these requirements, the proposed units would accord with the minimum unit size requirements. The minimum standards prescribed for individual rooms are set out within The London Housing Design

Guide and the proposed rooms conform to these standards as shown on the floor plans with the proposed units meeting the minimum requirement as follows:

- Dwelling No. 1 - 3 bedroom, 4 person = 84m² (110m² proposed)
- Dwelling No. 2 - 3 bedroom, 4 person = 84m² (110m² proposed)
- Dwelling No. 3 - 3 bedroom, 4 person = 84m² (110m² proposed)
- Dwelling No. 4 - 3 bedroom, 4 person = 84m² (90m² proposed)
- Dwelling No. 5 - 3 bedroom, 4 person = 84m² (89m² proposed)
- Coach House Dwelling - 4 bedroom, 8 person = 130m² (143m² proposed)

6.6.2 The proposed units would meet the relevant internal space standards for each sized unit. The 5 smaller dwellings would be single aspect, but split level and also south-west facing. They would also be shallow enough in plan to receive good levels of sunlight from the south and daylight from roof lights. The proposed couch house dwelling includes dual aspect views and also south and south west facing. All of the proposed units would provide sufficient levels of outlook from habitable rooms and daylight for future occupiers. Amenity areas are provided by way of courtyard garden areas and inset balconies at ground floor level.

6.6.3 It is noted that a reason for refusal of the previous application at the site related to living conditions for future occupiers due to the site not being suitably accessible. However, as highlighted above, this reason for refusal was not sustained as part of the appeal decision, where the appeal inspector considered that having regard to the aims of policy DM2 of the Councils Development Management DPD, *it would otherwise provide an acceptable standard of access and therefore is not in conflict with the policy as a whole. I therefore find that the conflict in this case would not be so harmful as to warrant the withholding of planning permission.* The proposal now provides an additional separate pedestrian access from Barratt Avenue. Given that the issues surrounding access would only improve from the determination of the previous application, such arrangements are acceptable.

6.7 Parking and highway safety

6.7.0 Local Plan (2017) Policy SP7 Transport states that the Council aims to tackle climate change, improve local place shaping and public realm, and environmental and transport quality and safety by promoting public transport, walking and cycling and seeking to locate major trip generating developments in locations with good access to public transport. This is supported by DM Policy (2017) DM31 'Sustainable Transport'.

6.7.1 DM Policy (2017) DM32 'Parking' states that the Council will support proposals for new development with limited or no on-site parking where there are alternative and accessible means of transport available, public transport accessibility is at least 4 as defined in the Public Transport Accessibility Index, a Controlled Parking

Zone (CPZ) exists or will be provided prior to the occupation of the development parking is provided for disabled people; and parking is designated for occupiers of developments specified as car capped.

- 6.7.2 It is noted that as part of the previously refused application at the site for 3 additional units, no objections were raised by the Council's Transport Officers regarding the development. The impact of that development was not considered to give rise to significant concerns in terms of parking pressure that would necessitate securing the development as car free.
- 6.7.3 The current scheme would provide 6 additional units, and would be subject to being designated as a car free development, secured by way of a section 106 agreement. Given this, in addition to the high public transport accessibility of the site (PTAL 5), future occupiers of the development are more likely to use sustainable modes of transport and that the development would not lead to a significant increase in parking pressure within the locality of the site.
- 6.7.4 It is noted that concerns have been raised in relation to safety / traffic concerns through the construction phase of the development. However, it is considered any potential issues arising from this could be adequately dealt with by way of condition ensuring that a detailed Construction Management Plan be submitted prior to works commencing on site. The Council's Transportation Team have been consulted on the application and no in principle objections have been raised, subject to the imposition of the aforementioned condition.

6.8 Energy and Climate Change

- 6.8.0 The NPPF and London Plan Policies 5.1, 5.2, 5.3, 5.7, 5.8, 5.9, 5.10 and 5.11, and Local Plan Policy SP4 sets out the approach to climate change and requires developments to meet the highest standards of sustainable design, including the conservation of energy and water; ensuring designs make the most of natural systems and the conserving and enhancing the natural environment. The London Plan requires all new homes to achieve a zero-carbon target beyond Part L 2013 of the Building Regulations.
- 6.8.1 New development is expected to achieve the necessary energy and CO2 requirements within the London Plan and Haringey Council's Local Plan or pay an offset payment. The applicant has submitted an Energy Statement Report and appendices with SAP calculations and a carbon emission reporting spreadsheet. The carbon savings under Be Lean are 23%, which is supported. The total emission savings are up to 60.2%. Whilst not zero carbon, this improvement is supported. In terms of overheating, a dynamic thermal assessment has not been provided. However, the Council's Carbon Management Team have been consulted as part of the application and consider that this element can be dealt with by way of condition. Given that the application relates to minor development, the proposal would not be subject to a carbon off-set contribution.

6.9 Flood Risk and Drainage

- 6.9.0 London Plan (2016) Policy 5.13 (Sustainable drainage) and Local Plan (2017) Policy SP5 (Water Management and Flooding) require developments to utilise Sustainable Urban Drainage Systems (SUDS) unless there are practical reasons for not doing so, and aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible in line with the drainage hierarchy.

Policy also requires drainage to be designed and implemented in ways that deliver other policy objectives, including water use efficiency and quality, biodiversity, amenity and recreation. Further guidance on implementing Policy 5.13 is provided in the Mayor's Sustainable Design and Construction SPG (2014) including the design of a suitable SUDS scheme. The site is located within Flood Zone 1 and is therefore considered to have a low probability of flooding. The applicant has not submitted a Flood Risk Assessment. However, the Council's Drainage Officer has been consulted as part of the development and the area isn't within a Critical Drainage Area, as designated by Policy DM26 of the Council's Development Management DPD. The site is classified as a low risk of flooding according to the Environment Agency maps and the Council's Drainage Officer has not raised any concerns, subject to a condition regarding the submission of a drainage strategy.

6.10 Trees and ecology

- 6.10.0 Haringey local Plan (2013) policy SP13 'Open Space and Biodiversity' requires that all new development shall protect and improve Haringey's parks and open spaces. The Council has a duty to have regard for conserving biodiversity and will not permit development on SINCS and LNRs unless there are exceptional circumstances and where the importance of any development coming forward outweighs the nature conservation value of the site. In such circumstances, or where a site has more than one designation, appropriate mitigation measures must be taken and, where practicable and reasonable, additional nature conservation space must be provided.

- 6.10.1 DM Policy (2017) DM1 'Delivering High Quality Design' states that the Council will expect development proposals to respond to trees on and close to the site.

- 6.10.2 It is noted that there are a number of trees on and adjacent to the site. None of the trees are designated under a Tree Preservation Order but are protected by virtue of being located within the Conservation Area.

- 6.10.3 The site is adjoined by a designated area of Significant Local Open Land (SLOL) and a Site of Importance for Nature Conservation (SINC) of Metropolitan Importance.

6.10.4 It is noted that as part of the assessment of the previously refused scheme at the site, it was highlighted that the site is not an intrinsically dark landscape as it is surrounded by residential properties to the north and south, with associated light coming from existing windows of neighbouring properties as well as street lighting. Mindful of this and the nature of the structures currently on site, the site has limited potential to support a bat population/ habitat. It is accepted that the trees next to the New River may provide a foraging habitat for bats. These trees are not affected by the proposal and therefore foraging routes through and next to the New River will not be affected here. A condition will be imposed in respect of lighting across the site.

6.10.5 A number of trees would need to be removed to facilitate the development. However, the trees to be removed from the site are generally category C trees, or below and of low amenity value. Three category B trees of reasonable amenity value would also need to be removed to facilitate the development. However, this would be subject to the re-planting of 25 trees within the site to off-set the loss of this vegetation. Trees adjacent to the site are to be retained and they would be protected throughout the construction of the development. This would be secured by way of condition to ensure adequate tree protection fencing is installed.

6.10.6 Subject to conditions, the proposal would not have a significant adverse impact upon in terms of loss of trees or biodiversity and would therefore be in accordance with the above policies.

6.11 Basement development

6.11.0 Development Management DPD (2017) policy DM18 sets out the Council's requirements for residential basement development, including new basements, extensions to existing basements and the creation of lightwells. All proposed basement development must be undertaken in a way that that does not harm the amenity of neighbours, compromise the structural stability of adjoining properties, increase flood risk or damage the character of the area or natural environments. A Basement Impact Assessment (BIA) containing all relevant information around potential impacts must be submitted as part of the application.

6.11.1 A BIA has been submitted as part of this application which shows that there is no risk of flooding from either surface water or from rivers or seas (including the New River) resulting from the excavation of the basements and lightwells that might affect future occupiers. The Council's Building Control Team have been consulted as part of the application and did not raise any objections subject to conditions. Similarly, the Council's Drainage Officer has raised no objections to the proposed development.

6.11.2 In terms of ground movements, the assessment shows that either none or very slight levels as most existing residential properties lie beyond the distance to no horizontal or vertical ground movement due to the basement excavations and wall

constructions. Calculations indicate that only the rear single storey extension to 19 Barratt Avenue may experience very slight hairline cracks that can be easily treated.

6.11.3 Given the separation involved, the recommendations outlined in the BIA should also be sufficient to further mitigate any residual risk. Moreover, the Party Wall Act and Building Regulations would provide further safeguards to identify and control the nature and magnitude of the effect on neighbouring properties. The necessary party-wall agreements with adjoining owners would need to be in place prior to commencement of works on site. The Party Wall Act 1996 exists separately from the planning system, to reconcile differences that adjoining development might cause.

6.11.4 In summary while it is recognised that certain aspects of the works here cannot be determined absolutely at the planning stage, the information submitted to the LPA to date does provide assurances that the works here can be carried out successfully without affecting adjoining properties. A condition will be imposed to ensure that the structural side of the basement is overseen by a suitably qualified chartered engineer.

6.12 Conclusion

6.12.0 The principle of the creation of additional family sized housing is considered acceptable. The development would comply with policy DM7, therefore the principle of backland development would be acceptable in this location. Based on the detailed design response, and taking into account improvements to open space, the heritage concerns arising from the previous appeal are considered to have been addressed. Taking into account all material considerations, the proposal is considered acceptable and is in accordance with policy and overcomes the previous reasons for refusal.

6.12.1 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION

7.0 CIL

Based on the information given on the plans, the Mayoral CIL charge will be £33,219.48 (557 sqm x £59.64) and the Haringey CIL charge will be £128,438.63 (557 sqm x £230.59). This will be collected by Haringey after/should the scheme is/be implemented and could be subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index. An informative will be attached advising the applicant of this charge.

8.0 RECOMMENDATIONS

GRANT PERMISSION subject to conditions subject to conditions in Appendix 1 and subject to sec. 106 Legal Agreement

Applicant's drawing No.(s) 001B, 002B, 003B, 101B, 301B, 302B, 401B, 402B, 800B, 900B, Arboricultural Impact Assessment, Arboricultural Method Statement, Basement Impact Assessment, Outline Construction Logistics Plan, Design & Access Statement, 1678-EX-001, 1678-EX-002, 1678-EX-003, 1678-EX-004, 1678-PA-010, 1678-PA-011, 1678-PA-012, 1678-PA-013, 1678-PA-014, 1678-PA-015, 1678-PA-016, 1678-PA-017, 1678-PA-017, 1678-PA-019, 1678-PA-020, 1678-PA-021, Energy Statement Report P03, Heritage Statement, Daylight Report Rev 2 & Transport Statement

Subject to the following condition(s)

Appendix 1

1. The development hereby authorised must be begun not later than the expiration of 3 years from the date of this permission, failing which the permission shall be of no effect.

Reason: This condition is imposed by virtue of the provisions of the Planning & Compulsory Purchase Act 2004 and to prevent the accumulation of unimplemented planning permissions.

2. The approved plans comprise drawing nos (001B, 002B, 003B, 101B, 301B, 302B, 401B, 402B, 800B, 900B, Arboricultural Impact Assessment, Arboricultural Method Statement, Basement Impact Assessment, Outline Construction Logistics Plan, Design & Access Statement, 1678-EX-001, 1678-EX-002, 1678-EX-003, 1678-EX-004, 1678-PA-010, 1678-PA-011, 1678-PA-012, 1678-PA-013, 1678-PA-014, 1678-PA-015, 1678-PA-016, 1678-PA-017, 1678-PA-017, 1678-PA-019, 1678-PA-020, 1678-PA-021, Energy Statement Report P03, Heritage Statement, Daylight Report Rev 2 & Transport Statement). The development shall be completed in accordance with the approved plans except where conditions attached to this planning permission indicate otherwise or where alternative details have been subsequently approved following an application for a non-material amendment.

Reason: In order to ensure the development is carried out in accordance with the approved details and in the interests of amenity.

3. Samples of materials to be used for the external surfaces of the development shall be submitted to, and approved in writing by, the Local Planning Authority before any above ground development is commenced including the following:
 - Detail design to scale 1:20 in plan section and elevation of proposed buildings and landscape

- Detail design to scale 1:20 in plan section and elevation of proposed architectural lighting and
- Details to scale 1:10 and material specification of windows, rooflights, doors, walls, stairs, roofs, green roofs, balustrades, finishes. All details both internal and external.
- Material samples of the above details to be submitted in the form of sample panels for approval. Samples should include sample panels or brick types and a roofing material sample combined with a schedule of the exact product references.

Reason: In order for the Local Planning Authority to retain control over the exact materials to be used for the proposed development and to assess the suitability of the samples submitted in the interests of visual amenity consistent with Policy 7.6 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2017 and Policy DM1 of The Development Management DPD 2017.

4. No demolition or development shall take place until a stage 1 written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. For land that is included within the WSI, no demolition or development shall take place other than in accordance with the agreed WSI, and the programme and methodology of site evaluation and the nomination of a competent person(s) or organisation to undertake the agreed works. If heritage assets of archaeological interest are identified by stage 1 then for those parts of the site which have archaeological interest a stage 2 WSI shall be submitted to and approved by the local planning authority in writing. For land that is included within the stage 2 WSI, no demolition/development shall take place other than in accordance with the agreed stage 2 WSI which shall include:
 - A. The statement of significance and research objectives, the programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works
 - B. Where appropriate, details of a programme for delivering related positive public benefits.
 - C. The programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material. this part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the stage 2 WSI.
5. Full details of the lighting across the site shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the approved development. The details shall include the location and full specification of all lamps; light levels/spill lamps, floodlights, support structures. The lighting measures shall be carried out strictly in accordance with the details so approved, shall be installed prior to occupation of the development and shall be maintained

as such thereafter. Reason: To ensure that any resulting general or security lighting is appropriately located, designed do not adversely impact neighbouring residential amenity and are appropriate to the overall design of the buildings as well as protecting the biodiversity value of the site.

6. No development shall take place until details of the type and location of secure and covered cycle parking facilities have been submitted to and approved in writing by the Local Planning Authority. The development shall not be occupied until a minimum of 12 no. cycle parking spaces for users of the development, have been installed in accordance with the approved details. Such spaces shall be retained thereafter for this use only.

Reason: To promote sustainable modes of transport in accordance with Policies 6.1 and 6.9 of the London Plan 2016 and Policy SP7 of the Haringey Local Plan 2017.

7. Details of a scheme for the storage and collection of refuse from the premises shall be submitted to and approved by the Local Planning Authority prior to the commencement of the use. The approved scheme shall be implemented and permanently retained to the satisfaction of the Local Planning Authority.

Reason: In order to protect the amenities of the locality and to comply with Policy DM4 of The Development Management DPD 2017 and Policy 5.17 of the London Plan 2016.

8. No development shall commence until a scheme for the treatment of the surroundings of the proposed development including the timescale for the planting of trees and/or shrubs, the maintenance of trees to be retained on site and appropriate hard and soft landscaping has been submitted to and approved in writing by the Local Planning Authority. The development hereby permitted shall be implemented in accordance with the approved details.

Reason: In order to provide a suitable setting for the proposed development in the interests of visual amenity consistent with Policy 7.21 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2017 and Policy DM1 of The Development Management DPD 2017.

9. No development shall take place, including any works of demolition, until a Construction Logistics Plan, to include details of:

- a) parking and management of vehicles of site personnel, operatives and visitors
- b) loading and unloading of plant and materials
- c) storage of plant and materials
- d) programme of works (including measures for traffic management)
- e) provision of boundary hoarding behind any visibility zones
- f) wheel washing facilities

- g) site investigation
- h) site specific boreholes; and
- l) basement construction method details

have been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented and retained during the demolition and construction period.

Reasons: To ensure there are no adverse impacts on the free flow of traffic on local roads and to safeguard the amenities of the area consistent with Policies 6.3, 6.11 and 7.15 of the London Plan 2011, Policies SP0 of the Haringey Local Plan 2017 and with Policy DM1 of The Development Management DPD 2017.

10. No works shall be carried out on site until a detailed Air Quality and Dust Management Plan (AQDMP), detailing the management of demolition and construction dust, has been submitted and approved in writing by the Local Planning Authority. The plan shall be completed in accordance with the GLA SPG Dust and Emissions Control and shall also include a Dust Risk Assessment.

Reason: To Comply with Policy 7.14 of the London Plan.

11. No development shall be carried out until such time as the person carrying out the work is a member of the Considerate Constructors Scheme and its code of practice, and the details of the membership and contact details are clearly displayed on the site so that they can be easily read by members of the public.

Reason: In the interests of residential amenity.

12. Before development commences other than for investigative work:

a) A desktop study shall be carried out which shall include the identification of previous uses, potential contaminants that might be expected, given those uses, and other relevant information. Using this information, a diagrammatical representation (Conceptual Model) for the site of all potential contaminant sources, pathways and receptors shall be produced. The desktop study and Conceptual Model shall be submitted to the Local Planning Authority. If the desktop study and Conceptual Model indicate no risk of harm, development shall not commence until approved in writing by the Local Planning Authority.

b) If the desktop study and Conceptual Model indicate any risk of harm, a site investigation shall be designed for the site using information obtained from the desktop study and Conceptual Model. This shall be submitted to, and approved in writing by, the Local Planning Authority prior to that investigation being carried out on site. The investigation must be comprehensive enough to enable:-

- i) a risk assessment to be undertaken,

ii) refinement of the Conceptual Model, and
iii) the development of a Method Statement detailing the remediation requirements. The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority for written approval.

c) If the risk assessment and refined Conceptual Model indicate any risk of harm, a Method Statement detailing the remediation requirements, using the information obtained from the site investigation, and also detailing any post remedial monitoring shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site.

Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety in accordance with Policy 5.21 of the London Plan 2016 and Policy DM23 of The Development Management DPD 2017.

13. Where remediation of contamination on the site is required completion of the remediation detailed in the method statement shall be carried out and a report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority, before the development is occupied.

Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety in accordance with Policy 5.21 of the London Plan 2011 and Policy DM1 of The Development Management DPD 2017.

14. Prior to the commencement of any development hereby approved and before any equipment, machinery or materials are brought onto the site for the purposes of the development hereby approved, a Tree Protection method statement incorporating a solid barrier protecting the stem of the trees and hand dug excavations shall be submitted to and approved in writing by the Local Planning Authority. The works shall be carried out as approved and the protection shall be maintained until all equipment, machinery and surplus materials have been removed from the site.

Reason: In order to ensure the safety and well-being of the trees adjacent to the site during constructional works that are to remain after works are completed consistent with Policy 7.21 of the London Plan 2016, Policy SP11 of the Haringey Local Plan 2017 and Policy DM1 of The Development Management DPD 2017.

15. Living Walls and Roofs

Prior to the commencement of development, details of the living roofs must be submitted to and approved in writing by the Local Planning Authority. The details shall include:

- (a) A roof plan identifying where the living roofs will be located;
- (b) A substrate of no less than 120mm for extensive living roofs, and no less than 250mm for intensive living roofs;
- (c) Sections showing the diversity of substrate depths and types across the roof to provide contours of substrate, such as substrate mounds in areas with the greatest structural support to provide a variation in habitat;
- (d) A plan showing the location of log piles / flat stones for invertebrates;
- (e) The range of native species of wildflowers and herbs planted to benefit native wildlife. The living roof will not rely on one species of plant life such as Sedum (which are not native);
- (f) Irrigation, management and maintenance arrangements.

The development shall be implemented in accordance with the approved scheme prior to its first occupation and the living roofs shall be retained and managed thereafter in accordance with the approved management arrangements. No alterations to the approved scheme shall be permitted without the prior written consent of the Local Planning Authority.

Reason: To ensure a sustainable development that provides provision towards the creation of habitats for biodiversity, mitigate against climate change and support water retention, consistent with Policy 5.11 of the London Plan 2016 and Policies SP4, SP5, SP11 and SP13 of the Haringey Local Plan 2017..

16. No part of the 'living roof' shall be used as an amenity area.

Reason: In order to protect the amenity of occupants of the adjoining residential properties consistent with Policy SP11 of the Haringey Local Plan 2017 and Policy DM1 of The Development Management DPD 2017.

17. Prior to occupation details of all enclosures around the site boundary (fencing, walling, openings etc) including measures to prevent impact on the Tunnel Gardens SINC and method of installation of boundary fences adjoining the New River SINC at a scale of 1:20, have been submitted to and approved in writing by the Local Planning Authority. Details shall include the proposed design, height and materials. The approved works shall be completed prior to occupation of the development and shall be permanently retained thereafter.

Reason: In the interest of public safety and security and to protect the visual amenity of the locality consistent with Policies 3.5, 7.4, 7.5 and 7.6 of the London Plan 2016 and Policy SP11 of the Haringey Local Plan 2017 and Policy DM1 of The Development Management DPD 2017.

18. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (and any order revoking and re-enacting the order) no extensions or outbuildings shall be built and no new window or door

openings inserted into any elevation of the buildings (other than that development expressly authorised by this planning permission).

Reason: To safeguard the amenities of neighbouring occupiers and the general locality.

19. The basement works hereby approved shall not commence until such time as a suitably qualified chartered engineer with membership of the appropriate professional body has been appointed to inspect, approve and monitor the critical elements of both permanent and temporary basement construction works throughout their duration to ensure compliance with the design which has been checked and approved by a building control body. Details of the appointment and the appointee's responsibilities shall be submitted to and approved in writing by the Council prior to the commencement of development. Any subsequent change or reappointment shall be confirmed forthwith and retained for the duration of the construction works.

Reason: To safeguard the appearance and structural stability of neighbouring buildings and the character of the immediate area in accordance with the requirements of policy.

20. No development shall take place other than site set up and demolition works, until a drainage strategy for the control of surface water has been submitted and approved in writing by the Local Planning Authority. The drainage strategy shall be constructed in accordance with the approved details and thereafter retained.

Reason: To promote a sustainable development consistent with Policies SP0, SP4 and SP6 of the Haringey Local Plan 2013.

21. Prior to the commencement of development, a detailed Overheating Assessment must be submitted and approved in writing by the Local Planning Authority and shall be informed by Dynamic Thermal Modelling based on CIBSE TM59 guidance and TM49 weather files for London's future weather/temperature projections. The assessment shall be undertaken in line with the following:

- The urban dataset for the three DSYs;
- Future weather patterns to projected impacts over the time periods 2020s, 2050s and 2080s, all time periods should be modelled. Mitigation for the 2020s period must be integrated into the design through passive design measures. The risks and the mitigation strategy for the periods of the 2050s and 2080s should be set out in a retrofit plan, confirming that measures can be fitted in the future and who will own the overheating risk;
- Floor plans highlighting the modelled dwellings across the development and showing all rooms (with unique reference number). The applicant is expected to model the following most likely to overheat dwellings:
 - At least 15% of all rooms across the development site;

- All single-aspect dwellings facing west, east, and south;
- At least 50% of rooms on the top floor;
- 75% of all modelled rooms will face South or South/west;
- Rooms closest to any significant noise and / or air pollution source, with windows closed at all times (unless they do not need to be opened and confirmed in the Noise and the Air Quality Assessments).

Any overheating mitigation measures set out in an approved Overheating Assessment shall be implemented before any of the dwellings in the Block to which they relate are first occupied and retained thereafter.

Reason: To enable the Local Planning Authority to assess overheating risk and to ensure that any necessary mitigation measures are implemented prior to construction, and maintained, in accordance with Policy 5.9 of the London Plan, Draft Policy SI4 of the draft New London Plan, and Policies SP4 and DM21 of the Local Plan.

22. The development hereby approved shall be constructed in accordance with the Energy Statement Report prepared by Delta Green (dated 24 September 2020, Rev P03) delivering a 60.2% improvement on carbon emissions over 2013 Building Regulations Part L with high fabric efficiencies and air source heat pumps (ASHPs).

Prior to construction, details of the proposed ventilation and heating systems shall be submitted to the Local Planning Authority. This must include:

- efficiency and location of the proposed Mechanical Ventilation and Heat Recovery (MVHR) and ASHPs, with plans showing the rigid MVHR ducting and ASHP pipework;
- proposed noise and visual mitigation measures for the ASHP;
- evidence that the ASHP complies with other relevant issues as outlined in the Microgeneration Certification Scheme Heat Pump Product Certification Requirements.

Reason: To ensure the development can comply with the Energy Hierarchy in line with London Plan 2016 Policy 5.2, draft New London Plan (Intend to Publish) Policy SI2 and Local Plan Policy SP4.

23. Prior to the first occupation of the development, a full Service and Delivery Plan (SDP) shall be submitted in writing to and for approval by the Local Planning Authority. The service and delivery plan must also include the restricted hours of delivery as agreed by the Council's parking management team. The plan shall be implemented as approved and maintained thereafter unless agreed in writing by the Local Planning Authority.

REASON: To protect amenity, reduce congestion and mitigate obstruction to the flow of traffic.

Informatives:

INFORMATIVE: In dealing with this application, Haringey Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) (Amendment No.2) Order 2012 to foster the delivery of sustainable development in a positive and proactive manner.

INFORMATIVE: CIL

Based on the information given on the plans, the Mayoral CIL charge will be £33,219.48 (557 sqm x £59.64) and the Haringey CIL charge will be £128,438.63 (557 sqm x £230.59). This will be collected by Haringey after/should the scheme is/be implemented and could be subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index. An informative will be attached advising the applicant of this charge.

INFORMATIVE :

Hours of Construction Work: The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:-

- 8.00am - 6.00pm Monday to Friday
- 8.00am - 1.00pm Saturday
- and not at all on Sundays and Bank Holidays.

INFORMATIVE: Party Wall Act: The applicant's attention is drawn to the Party Wall Act 1996 which sets out requirements for notice to be given to relevant adjoining owners of intended works on a shared wall, on a boundary or if excavations are to be carried out near a neighbouring building.

INFORMATIVE: The new development will require numbering. The applicant should contact the Local Land Charges at least six weeks before the development is occupied (tel. 020 8489 5573) to arrange for the allocation of a suitable address.

INFORMATIVE: The London Fire Brigade strongly recommends that sprinklers are considered for new developments and major alterations to existing premises, particularly where the proposals relate to schools and care homes. Sprinkler systems installed in buildings can significantly reduce the damage caused by fire and the consequential cost to businesses and housing providers, and can reduce the risk to life. The Brigade opinion is that there are opportunities for developers and building owners to install sprinkler systems in order to save money, save property and protect the lives of occupier.

INFORMATIVE:

With regards to surface water drainage, it is the responsibility of a developer to make proper provision for drainage to ground, water course, or a suitable sewer. In respect of surface water, it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.

INFORMATIVE: Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

Appendix 2 Consultation Responses from internal and external agencies

Stakeholder	Question/Comment	Response
INTERNAL		
Transportation	<p>Access arrangements As a car free development, there will be foot and cycle access only to the houses, from the track to the side of No. 138 Station Road, and from another foot connection off Barratt Avenue. It is assumed a keyfob security system or similar will be in place.</p> <p>It is detailed that fire tender access will not be required as a sprinkler system will be installed in the new units, the emergency services will have to confirm their approval or the proposed arrangements.</p> <p>Car Parking considerations This is proposed as a car free development. It has very good accessibility to public transport services, and it would be appropriate as a permit free site. If granted consent, the applicant will have to enter into the appropriate planning agreement to meet the Council's costs of formalising the permit free status and prevent the new occupiers from obtaining CPZ permits.</p> <p>Whilst appropriate for the status of a car free/permit free site, all of the units are family sized units so there is potential for some additional car parking demands being generated. The applicant has included within their TA details of a Parking Stress Survey carried out during February 2020, and this recorded parking stresses at 60% within Station Road, with 6 spaces available out of the 15 in the closest proximity to the site. Parking pressures were high within Barratt Avenue with only two spaces available.</p> <p>2011 census recorded average car ownership at around 0.55 vehicles per household, and considering this information is now 10 years old, the permit free status, and very good access to public transport services, it is considered that any new parking demands arising from the site will be minimal and should not create any adverse parking impacts.</p>	<p>Comments noted and condition attached in relation to the submission of a Construction Management Plan, cycle storage, refuse storage and delivery and servicing plan.</p> <p>The development would be subject to a S106 agreement relating to restricting future occupiers obtaining car parking permits and car club provision.</p>

Stakeholder	Question/Comment	Response
	<p>It is suggested also that a car club facility be provide and funded by the developer, given these are all family sized houses. The applicant should liaise and consult with the car club operator and obtain their recommendations for provision for these units. It is expected that their recommendation will be for three years membership and a £50 driving credit per household. Provision of a car club facility for the development will reduce the likelihood of parking demands arising from the development and can be included within the S106.</p> <p>Cycle parking It is noted that two cycle parking spaces are proposed for each unit to be incorporated into the houses themselves. In principle this is fine subject to the proposed arrangements having sufficient security and space to easily and comfortably store two cycles. Full details are required for review and approval prior to commencement of the works and this can be covered by condition.</p> <p>Two visitor cycle parking spaces are proposed for location in the small green area to the south of the new units. Given there will be some form of keyfob security or similar this should be fine with respect to visitor cycle parking security.</p> <p>Delivery and servicing arrangements In total it is proposed that there would be on average one servicing/delivery trip to the houses per day. In reality this could be higher than that, however in any event, there will be opportunities for delivery/service vehicles to park and well within CPZ bays for up to 20 minutes to make deliveries so this is not expected to be problematical.</p> <p>Refuse and recycling arrangements A bin store is proposed for location along the main access track from Station Road to the site. It is detailed that bins here will be within the requisite maximum pull distances permitted by the Council (10m to the highway).</p> <p>Construction Phase Transportation have already commented on the submitted CLP and</p>	

Stakeholder	Question/Comment	Response
	<p>the separate comments are added to this response. There are details still required for the CLP that need to be provided for review and this should be conditioned for approval prior to commencement of the construction works to ensure safe and workable arrangements are in place.</p> <p>Summary This application is for redevelopment of the land behind Nos. 124 to 138 Station Road, to construct 6 new residential dwellings and retain and refurbish No. 140 Station Road. It is proposed as a car free development. In principle this is acceptable and appropriate, subject to the development being formally designated as car free, and the provision of a car club facility, both to be covered within the S106. In addition to this the developer will need to fund the reinstatement of the redundant crossover and footway and should enter into the appropriate agreements to do so. Again, this can be covered via the S106. Finally, full details of the cycle parking arrangements will be required for review and approval prior to commencement of the works for the development and this can be covered by condition along with a completed final version of the Construction Logistics Plan.</p> <p>Subject to the above condition and S106 contributions, Transportation does not object to the application.</p> <p>The following comments relate to the CLP included in the application.</p> <p>The overall aims of the applicant's CLP are as follows;</p> <ul style="list-style-type: none"> • To ensure construction vehicles are timed such that only one attends the Site at any one time. • To ensure no construction vehicles will load on-street with all accommodated within off-street loading facilities. • To ensure pedestrian and cyclist safety is maintained at all times along Station Road <p>The submitted CLP is a draft pending appointment of a contractor for the construction work for the development. Transportation have</p>	

Stakeholder	Question/Comment	Response
	<p>reviewed it and have the following comments;</p> <ul style="list-style-type: none"> • An 18 month build out is proposed, a programme will be required that details the durations of the different phases of the work (demolition, foundations, main build etc.) • For the initial demolition phase, skip lorries and any associated construction vehicles for that phase will need to reverse into the site under banksmen supervision to enable exit in a forward gear. • Upon completion of the demolition it is detailed all vehicles will be able to enter and exit in a forward gear. • The largest vehicle proposed to access the site is a readymix lorry, 2.39 metres wide and 8.36m long. • Whilst swept path plots have been provided showing vehicles are able to make manoeuvres, the swept paths plots do not appear to have 300mm safety buffers included. More details should be provided as to the clear widths available at the site access adjacent to No. 138 Station Road and along the access track into the site. • There are no details of the numbers of construction vehicles arrivals and departures on a daily/weekly basis. The document details this information can be provided in an updated CLP upon appointment of a main contractor. • It is commented that 'best endeavours' will be employed to avoid arrivals and departures during the 08:00-09:00 and 15:00-16:00 periods. These periods should be expanded to 0800 – 0930 and 1500 – 1630. There is also reference to demolition vehicles only arriving or departing during the 0930 – 1430 period. The regime of permitted arrival and departure times should be clarified to avoid peaks and school day start and finish periods so the 0930- 1500 period seem most appropriate. 	

Stakeholder	Question/Comment	Response
	<ul style="list-style-type: none"> • There is reference to scheduling of vehicle arrivals and departures, there will need to be a managed slot booking system employed by the site to avoid construction related vehicles waiting on the highway and adhere to the time periods referred to above. • The use of the northern side of Station Road for vehicle waiting/holding is proposed for up to 40 minutes. In principle vehicle waiting/holding should not be happening on the Highway, the scheduling/slot system should ensure this does not happen. The proposed location, if it is the short length of Single Yellow line close to the site, has access points for Thames Water and the Electrical utility company, and parked lorries at this location would also block the advisory cycle lane towards Wood Green Town Centre. So, this would not be acceptable. If the applicant wishes to utilise holding areas of any sort, they need to agree anything proposed for the public highway with the Borough's Network Management Team in the first instance and there is no guarantee they will be amenable to any proposals. <p>Summarising, whilst the draft CLP does provide some useful information, a finalised version should be provided for review and approval prior to commencement of the works. In particular, for the following;</p> <ul style="list-style-type: none"> • Clarification of the access widths at the narrowest points and along the access track • Provision of swept path plots with 300mm safety buffers to confirm that the proposed regime of vehicles serving the site is appropriate given the width available for access • Clarification of the regime for permitted hours for arrivals and departures • Confirmation that no vehicles will wait on the highway. And provision of agreed details for any vehicle holding/waiting arrangements 	

Stakeholder	Question/Comment	Response
	<ul style="list-style-type: none"> • A phased programme for the build out plus confirmation of the numbers of construction vehicle arrivals and departures to and from the site on a daily/weekly basis • Conformation of the arrangements for ensuring no debris or dust appears on the highway and associated wheel washing/highway inspection/cleaning regime proposed. <p>Upon sight of the updated CLP Transportation can review.</p>	
Building Control	This BiA is very much a desktop study, there is no site investigation, no site specific boreholes and no construction details – all of which could be pre commencement conditions.	Noted and conditions attached requiring a site investigation, site specific boreholes and construction details.
Conservation	<p>The development site lies within Wood Green Common Conservation Area, in close proximity to the New River, to Avenue Gardens and to the Common and is significantly constrained from the residential terraces which were erected between the end of the 19th century and early 20th century respectively along Barratt avenue and Station Road.</p> <p>Since then, the site has been framed to the north and south by the back gardens of the terraces and seems to have been independently used.</p> <p>It has an almost triangular shape with an east-west orientation and is accessed from Station Road via an entrance route which runs along the west flank of the end of terrace at No 138. The route leads to the back of the terrace, where there is a two storey brick building probably built at the same time as the terrace along Station road, as the historic 1914 OS map seems to suggest.</p> <p>The site is currently in poor conditions and cluttered with dense, overgrown vegetation and a number of run-down sheds which detract from the character and quality of the Conservation Area. It requires enhancement and also provides an opportunity for development, being very close to the metropolitan centre, amenities and public transport connections of Wood Green.</p>	Noted and conditions attached requiring materials and detail specifications to be submitted prior to the commencement of works.

Stakeholder	Question/Comment	Response
	<p>Within this context it is now proposed to demolish the existing structures, including the brick building by the access route and erect 6 dwellings sunk in a landscaped area and complemented by a community garden.</p> <p>The proposal has been developed in consultation with both conservation and urban design officers and originates from a comprehensive design exploration based on clear understanding of the green and visually open character of the site as well as from full appreciation of its spatial and visual relationship with the back gardens of the surrounding residential terraces.</p> <p>The proposed development is purposely founded on a lower level than the existing residential terraces so to respect and retain the primacy of the surrounding terraces and is totally integrated in its landscape, so to not overwhelm the existing rear gardens and so to not detract from the views from the rear elevations of the terraces.</p> <p>The scheme aims to provide the highest level of integration possible with the natural and built landscape of the Conservation Area and is designed to retain the green, open and self-contained character of the site as well as improving its landscaped and built quality and the views from the surrounding houses into the site.</p> <p>The curved forms of the proposed development, the undulate building line and the green roofs aim to mitigate the impact of new development on the surrounding private gardens and create a coherent site experience together with the proposed community garden on site.</p> <p>It may be useful to note that the existing 'coach house', which seems a very utilitarian, ancillary building, sits in a secluded location far from the street-front and is constrained in the north-west corner of the development site. Besides being an old building it is not identified or designated as a heritage asset or positive contributor to the Conservation Area, which would require at least a degree of architectural and historic interest or townscape merit, and there is no presumption for said structure to be retained. This application correctly identifies the heritage assets impacted and</p>	

Stakeholder	Question/Comment	Response
	<p>sensitively turns a neglected interstitial site into much needed residential accommodation and public garden while preserving the landscaped qualities of the site and of the Conservation Area.</p> <p>The proposed development is considered to be of appropriate scale, massing and architectural quality and would preserve the character and appearance of the conservation area without cause any harm, actually improving its quality and is therefore supported form conservation grounds depending on approval of the following:</p> <ul style="list-style-type: none"> • Detail design to scale 1:20 in plan section and elevation of proposed buildings and landscape • Detail design to scale 1:20 in plan section and elevation of proposed architectural lighting and • Details to scale 1:10 and material specification of windows, rooflights, doors, walls, stairs, roofs, green roofs, balustrades, finishes. All details both internal and external. • Material samples of the above details to be submitted in the form of sample panels for approval. 	
Design Officer	<p>Design Approach</p> <p>The five 2 storey terraced dwellings with undulating meadow roof is set into the ground to reduce the height and impact on surrounding properties by appearing single storey.</p> <p>The residential offer is modern and high quality, each home has dedicated cycle storage, promoting active travel, and the larger three of the five houses have a separate office and a small amount of desk space for home working.</p> <p>Each home has a generous amount of private external amenity, a hard-landscaped terrace accessible from the bedrooms at lower ground level.</p> <p>Each of the terraced homes is single aspect, however shallow enough in plan to receive good levels of sunlight from the south and daylight from roof lights. The amount of overhang fluctuates giving each home a different quality of light. It is advised that the levels of</p>	<p>Noted and conditions attached requiring details of materials, landscaping and management and green roof details.</p> <p>Conservation Officer has not objected to the demolition of the Coach House building.</p> <p>Daylight assessment provided demonstrates that the proposed units would receive sufficient levels of daylight.</p>

Stakeholder	Question/Comment	Response
	<p>light are somewhat equalised between the homes, and that some daylight testing should be carried out on the design to better appreciate the impact of the roof design.</p> <p>The existing structure of the existing two storey coach house could be retained and refurbished to anchor the new development in the existing context and reduce the amount of demolition on site. An investigation into the condition of existing buildings should be carried out before opting to demolish. As a new two storey structure it does not follow the same architectural logic of the proposed terrace, and appears over-scaled and insensitive to the site. The current office/studio use of this building is already suitable for the site, and there should be a consideration to retain it to create a more balanced, mixed small development.</p> <p>Rainwater harvesting recommended in BIA - could the applicant provide clarification of how this will be actioned through the design?</p> <p>Landscape 15 trees on the site will be lost to accommodate the development, however the proposed comprehensive soft and hard landscape strategy introduces a minimum of 3 new trees will be planted to replace the 3 felled category B trees. In addition, diverse plant species will be added to the existing natural character of the site, as well as the public benefit of a communal garden and additional high quality homes.</p> <p>Intensifying planting on the site's boundary gives natural screening and enhanced visual amenity to the surrounding neighbours, reducing the potential visual impact of the new development on existing residents.</p> <p>The introduction of water to the site with the pond and rill will bring in new wildlife and enhance the site to be used and enjoyed by children in particular. With the proposed safety grids in place this could be a pleasant, child friendly space for the local communities to use. The felled trees could be reused on site as interpretive play</p>	

Stakeholder	Question/Comment	Response
	<p>structures or the timber could be otherwise repurposed within the design.</p> <p>The landscape maintenance plan and management schedule are very clear and ensure that the publicly accessible areas of the site remain clean and well looked after.</p> <p>Materials The proposed hard landscaping materials are permeable, robust and durable elements that should weather well with low maintenance requirements, such as Corten steel for raised planters, gabion walls, and the Grasscrete provides a permeable, and visually fitting surfacing for vehicular access. The overall palette of materials is high quality and well balanced, integrating well with the natural environment.</p> <p>The applicant demonstrates that the Bauder meadow roof system has been used successfully in other precedent projects, and that the team have experience delivering this type of construction in their portfolio.</p>	
Carbon Management	<p>On 25/09/2020, the applicant submitted a revised Energy Statement Report (dated 24 September 2020, Rev P03) and appendices with SAP calculations and the carbon emission reporting spreadsheet.</p> <p>Sustainability No response has been provided in relation to the sustainability points made above, this aspect of the scheme is still not supported in principle.</p> <p>Be Lean It is good to see the fabric has been improved in response to the earlier comments. The carbon savings under Be Lean are now 23%, which is supported.</p> <p>Proposed fabric properties have been improved to: Floor u-value 0.13 W/m2K</p>	<p>Noted and conditions attached requiring details of green roofs, an overheating statement, and ventilation and heating systems to be submitted. The proposal is to be built in accordance with the submitted energy strategy.</p> <p>The development would not be subject to a carbon offset contribution as it does not relate to a major development and therefore does not meet this requirement.</p>

Stakeholder	Question/Comment	Response														
	<p>External wall u-value 0.13 W/m2K Roof u-value 0.13 W/m2K Door u-value 1.40 W/m2K Window u-value 1.40 W/m2K G-value0.76-0.80 Air permeability rate 3 m3/hm2 @ 50Pa MVHR efficiency 89%</p> <p>Carbon offset contribution The total emission savings have been increased to 60.2%. Whilst not zero carbon, this improvement is supported. A carbon offset contribution will be due for this scheme.</p> <table><tr><td>tCO2</td><td>%</td></tr><tr><td>Baseline emissions</td><td>12.23</td></tr><tr><td>Be Lean savings</td><td>2.81 23%</td></tr><tr><td>Be Clean savings</td><td>0 0%</td></tr><tr><td>Be Green savings</td><td>4.55 37.2%</td></tr><tr><td>Cumulative savings</td><td>7.36 60.2%</td></tr><tr><td>Carbon shortfall to offset (tCO2)</td><td>4.87</td></tr></table> <p>The indicative carbon offset contribution will be £13,879.50. [Based on 4.87 tCO2 x £95 x 30 years]</p> <p>Overheating The applicant submitted the Domestic Overheating Checklist, which is useful to see. However, a dynamic thermal assessment still needs to be undertaken to demonstrate the risk of overheating has been minimised. If this cannot be provided prior to determination, this report should be conditioned.</p> <p>Planning conditions</p> <p>Overheating Prior to the commencement of development, a detailed Overheating Assessment must be submitted and approved in writing by the</p>	tCO2	%	Baseline emissions	12.23	Be Lean savings	2.81 23%	Be Clean savings	0 0%	Be Green savings	4.55 37.2%	Cumulative savings	7.36 60.2%	Carbon shortfall to offset (tCO2)	4.87	
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Stakeholder	Question/Comment	Response
	<p>Local Planning Authority and shall be informed by Dynamic Thermal Modelling based on CIBSE TM59 guidance and TM49 weather files for London's future weather/temperature projections. The assessment shall be undertaken in line with the following:</p> <ul style="list-style-type: none"> - The urban dataset for the three DSYs; - Future weather patterns to projected impacts over the time periods 2020s, 2050s and 2080s, all time periods should be modelled. Mitigation for the 2020s period must be integrated into the design through passive design measures. The risks and the mitigation strategy for the periods of the 2050s and 2080s should be set out in a retrofit plan, confirming that measures can be fitted in the future and who will own the overheating risk; - Floor plans highlighting the modelled dwellings across the development and showing all rooms (with unique reference number). The applicant is expected to model the following most likely to overheat dwellings: <ul style="list-style-type: none"> - At least 15% of all rooms across the development site; - All single-aspect dwellings facing west, east, and south; - At least 50% of rooms on the top floor; - 75% of all modelled rooms will face South or South/west; - Rooms closest to any significant noise and / or air pollution source, with windows closed at all times (unless they do not need to be opened and confirmed in the Noise and the Air Quality Assessments). <p>Any overheating mitigation measures set out in an approved Overheating Assessment shall be implemented before any of the dwellings in the Block to which they relate are first occupied and retained thereafter.</p> <p>Reason: To enable the Local Planning Authority to assess overheating risk and to ensure that any necessary mitigation measures are implemented prior to construction, and maintained, in accordance with Policy 5.9 of the London Plan, Draft Policy SI4 of the draft New London Plan, and Policies SP4 and DM21 of the Local Plan.</p> <p>Energy Strategy</p>	

Stakeholder	Question/Comment	Response
	<p>The development hereby approved shall be constructed in accordance with the Energy Statement Report prepared by Delta Green (dated 24 September 2020, Rev P03) delivering a 60.2% improvement on carbon emissions over 2013 Building Regulations Part L with high fabric efficiencies and air source heat pumps (ASHPs).</p> <p>Prior to construction, details of the proposed ventilation and heating systems shall be submitted to the Local Planning Authority. This must include:</p> <ul style="list-style-type: none"> - efficiency and location of the proposed Mechanical Ventilation and Heat Recovery (MVHR) and ASHPs, with plans showing the rigid MVHR ducting and ASHP pipework; - proposed noise and visual mitigation measures for the ASHP; - evidence that the ASHP complies with other relevant issues as outlined in the Microgeneration Certification Scheme Heat Pump Product Certification Requirements. <p>Reason: To ensure the development can comply with the Energy Hierarchy in line with London Plan 2016 Policy 5.2, draft New London Plan (Intend to Publish) Policy SI2 and Local Plan Policy SP4.</p> <p>Living roofs</p> <p>Prior to the commencement of development, details of the living roofs must be submitted to and approved in writing by the Local Planning Authority. The details shall include:</p> <ul style="list-style-type: none"> (a) A roof plan identifying where the living roofs will be located; (b) A substrate of no less than 120mm for extensive living roofs, and no less than 250mm for intensive living roofs; (c) Sections showing the diversity of substrate depths and types across the roof to provide contours of substrate, such as substrate mounds in areas with the greatest structural support to provide a variation in habitat; (d) A plan showing the location of log piles / flat stones for invertebrates; (e) The range of native species of wildflowers and herbs 	

Stakeholder	Question/Comment	Response
	<p>planted to benefit native wildlife. The living roof will not rely on one species of plant life such as Sedum (which are not native); (f) Irrigation, management and maintenance arrangements.</p> <p>The development shall be implemented in accordance with the approved scheme prior to its first occupation and the living roofs shall be retained and managed thereafter in accordance with the approved management arrangements. No alterations to the approved scheme shall be permitted without the prior written consent of the Local Planning Authority.</p> <p>Reason: To ensure a sustainable development that provides provision towards the creation of habitats for biodiversity, mitigate against climate change and support water retention, consistent with Policy 5.11 of the London Plan 2016 and Policies SP4, SP5, SP11 and SP13 of the Haringey Local Plan 2017.</p>	
Drainage Officer	<p>I do apologise for the delay responding to you, I did initially start to review the BIA, report where the drainage information can be found, as the site is minor the LLFA, wouldn't normally provide comments and a Flood Risk Assessment is not required.</p> <p>Based on the information in the report, the area isn't in a CDA, and is classified as a low risk of flooding according to the Environment Agency maps. The applicant could provide more detail how the surface water would be dealt with on the site, there's no information on existing runoff rates or proposed discharge rates, this could be provided on the Haringey, pro-forma, supported by a drainage strategy and drawings.</p> <p>The report didn't raise anything that would cause concern for us. Please let me know if you need anything else from us at this stage.</p> <p>If you do include a condition, it could be based around the following: - no development shall take place other than site set up and demolition works, until a drainage strategy for the control of surface water has been submitted and approved in writing by the LPA?.</p>	Noted and a condition requiring details of a drainage strategy is to be attached.

Stakeholder	Question/Comment	Response
EXTERNAL		
Historic England	<p>The above case has been brought to my attention by a local resident. The scheme falls just outside the Wood Green Archaeological Priority Area but in view of the bulk excavation proposed for the development, and the wide impact on any buried remains that would arise, I offer the following advice. My advice is informed by the applicant's heritage statement and the study produced by Mr Colin Kerr.</p> <p>Past archaeological investigation in the area has been very limited and thus far I am not aware of any fieldwork projects seeking to elucidate Wood Green's past. There are a handful of records of spotfinds of prehistoric material in the wider landscape and the local settlements may have mediaeval or even Saxon roots. The north west edge of the site is bounded by the original course of the New River before its route was shortened in the 1850s.</p> <p>I am grateful for Mr Kerr's reproduction of the 1619 Dorset Plan, which shows a building on the application site and this may be the same building also shown on the First Edition OS plan before it was demolished in the late nineteenth century and which is referred to as The Grange in the material. This building and its neighbours faced the green of Wood Green itself and, in common with settlement patterns elsewhere in the borough as well as in LBs Enfield and Waltham Forest, this position may represent a historic settlement focus common to the area.</p> <p>The bulk excavation proposed to develop the site would result in the removal of any buried archaeological remains, including any remnants of the seventeenth century building.</p> <p>Should the LPA grant consent for the scheme, I recommend that the following condition be added to any forthcoming consent in order to identify any remains and then appropriately manage harm to them, through investigation and improved public understanding of the area's heritage:</p>	

Stakeholder	Question/Comment	Response
	<p>CONDITION:</p> <p>No demolition or development shall take place until a stage 1 written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. For land that is included within the WSI, no demolition or development shall take place other than in accordance with the agreed WSI, and the programme and methodology of site evaluation and the nomination of a competent person(s) or organisation to undertake the agreed works.</p> <p>If heritage assets of archaeological interest are identified by stage 1 then for those parts of the site which have archaeological interest a stage 2 WSI shall be submitted to and approved by the local planning authority in writing. For land that is included within the stage 2 WSI, no demolition/development shall take place other than in accordance with the agreed stage 2 WSI which shall include:</p> <p>A. The statement of significance and research objectives, the programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works</p> <p>B. Where appropriate, details of a programme for delivering related positive public benefits.</p> <p>C. The programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material. this part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the stage 2 WSI.</p> <p>Informative:</p> <p>Written schemes of investigation will need to be prepared and implemented by a suitably qualified professionally accredited archaeological practice in accordance with Historic England's Guidelines for Archaeological Projects in Greater London.</p>	

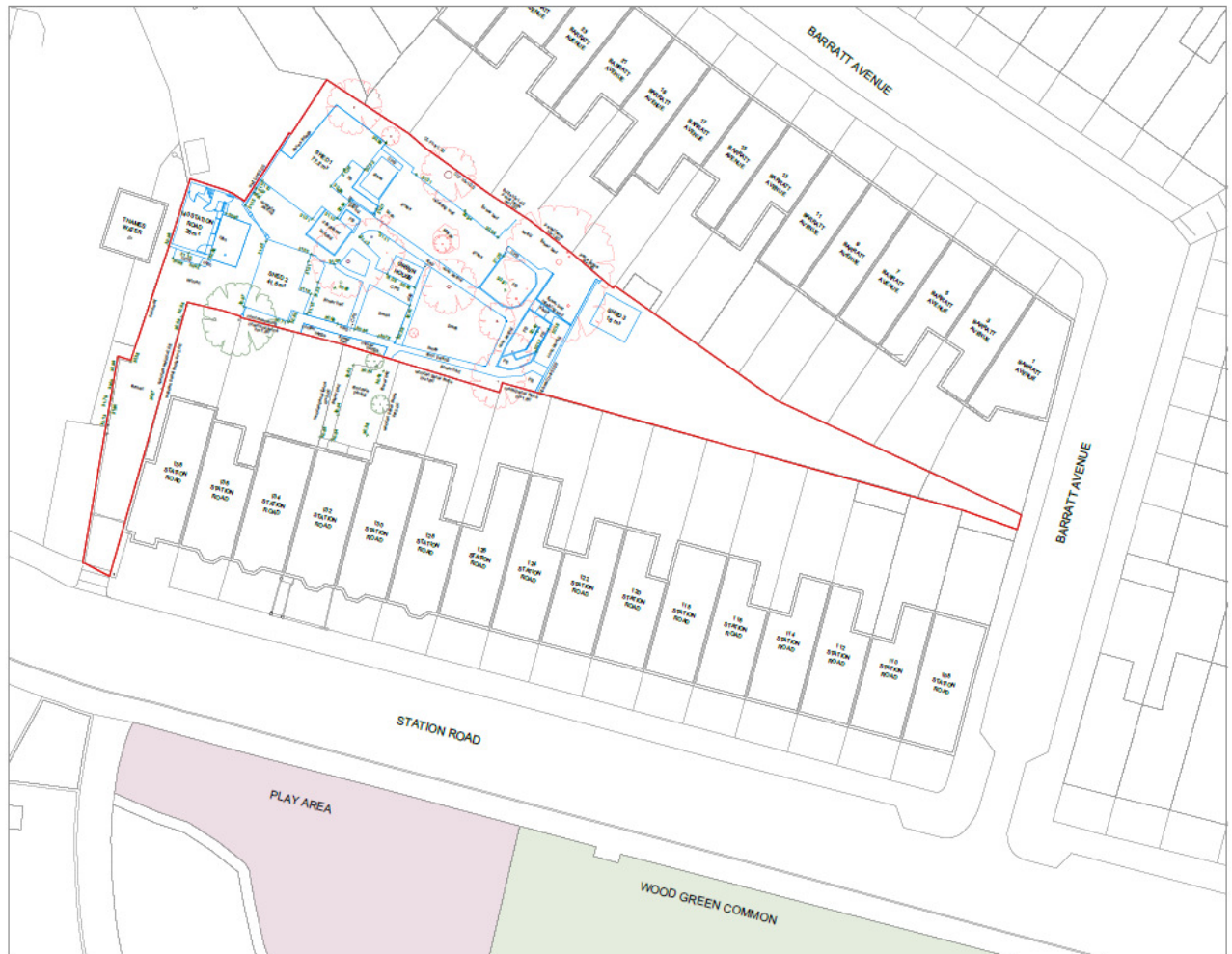
Stakeholder	Question/Comment	Response
Local Representations		
Cllr Peter Mitchell	<p>In line with the Planning Protocol (section 2.21), I would like to request that the application HGY/2020/1841, land at the rear of 132 Station Road N22, is referred to the Planning Committee for determination if officers are recommending it for approval.</p> <p>This is a significant backland development and there is likely to be substantial local interest, as there was for the previous application, HGY/2017/2182, which was referred to the Planning Committee. The application was refused by the Committee and an appeal was dismissed.</p> <p>The previous application was for 3 houses, while this latest one is for 6 houses, though this does include the demolition of an existing building which was not part of the previous application.</p> <p>I have already been approached by residents asking if this can go to the Planning Committee.</p>	
NEIGHBOURING PROPERTIES		
	<ol style="list-style-type: none"> 1 Housing needs are already being met 2 Noise and disturbance 3 Increase in traffic 4 Out of character with the open space / conservation area 5 Light pollution 6 Loss of biodiversity / wildlife / protected species 7 Safety concerns during construction 8 Materials at odds with conservation area 	<p>1. Government policy as set out in the National Planning Policy Framework 2019 requires Local Planning Authorities to significantly boost the supply of housing. The 2020 Housing Delivery Test (HDT) results were published on 19 January 2021 and as a result the LPA is now a “presumption authority” and paragraph 11d of the NPPF is relevant. Given the Council’s delivery of housing over the last three years is substantially below its housing target and so paragraph 11d) of the NPPF is engaged by virtue of footnote 7 of the NPPF.</p> <p>2. This is a residential area. Proposed additional residential dwellings would not lead to noise creation harmful to the amenity of neighbouring residents.</p>

Stakeholder	Question/Comment	Response
	<p>9 Plumbing and drainage issues</p> <p>10 Overlooking and loss of privacy</p> <p>11 Re-development of existing building on site overbearing</p> <p>12 Loss of employment</p> <p>13 Overdevelopment of the site</p> <p>14 Impact from the basement</p> <p>15 Security issues</p> <p>16 Archaeological impacts</p>	<p>3. Officers are of the opinion that the scheme would not result in an increase in parking demand that would have an adverse impact upon supply of on street parking within the local area.</p> <p>4. The design of the proposed dwellinghouses is considered to be acceptable and would not harm visual amenity or the character and appearance of the conservation area.</p> <p>5. The potential for light pollution is not considered to be harmful.</p> <p>6. The site is not a designated site for Nature Conservation and the impact on local ecology is not considered to be harmful.</p> <p>7. LBH Transportation have been consulted and consider that these issues can be dealt mitigated against by the submission of a Construction Management Plan.</p> <p>8. The provisional details of materials are considered to be of high quality. More detailed information regarding materials is to be submitted as part of a condition.</p> <p>9. The site is not within a Critical Drainage Area. The Council's Drainage Officer has been consulted and raised no in principle objections. A condition is to be attached requiring a drainage strategy be submitted prior to works commencing on site.</p> <p>10. The development is not considered to result in unacceptable levels of overlooking or loss of privacy to neighbouring occupiers.</p>

Stakeholder	Question/Comment	Response
		<p>11. The buildings would be relatively low in height and would not result in an unacceptable overbearing impact.</p> <p>12. The proposal would not involve the loss of any designated employment land or floorspace. The loss of the use of the existing building would not be significant in employment terms. The proposed use would be residential and more appropriate land use for the locality.</p> <p>13. Site coverage and layout is considered to be acceptable.</p> <p>14. A Basement Impact Assessment (BIA) has been submitted in line policy and no significant harm to surrounding buildings was identified. No further technical evidence has been submitted to refute the findings of the BIA.</p> <p>15. The site is a vacant backland plot. It is considered that security would be improved given increased passive surveillance resulting from occupation.</p> <p>16. The site is not located within a designated area for Archaeological importance. Comments have been received by GLAAS of Historic England and suggested conditions have been attached in relation to a written scheme of investigation.</p>

Appendix 3 Plans and Images

Site Location Plan

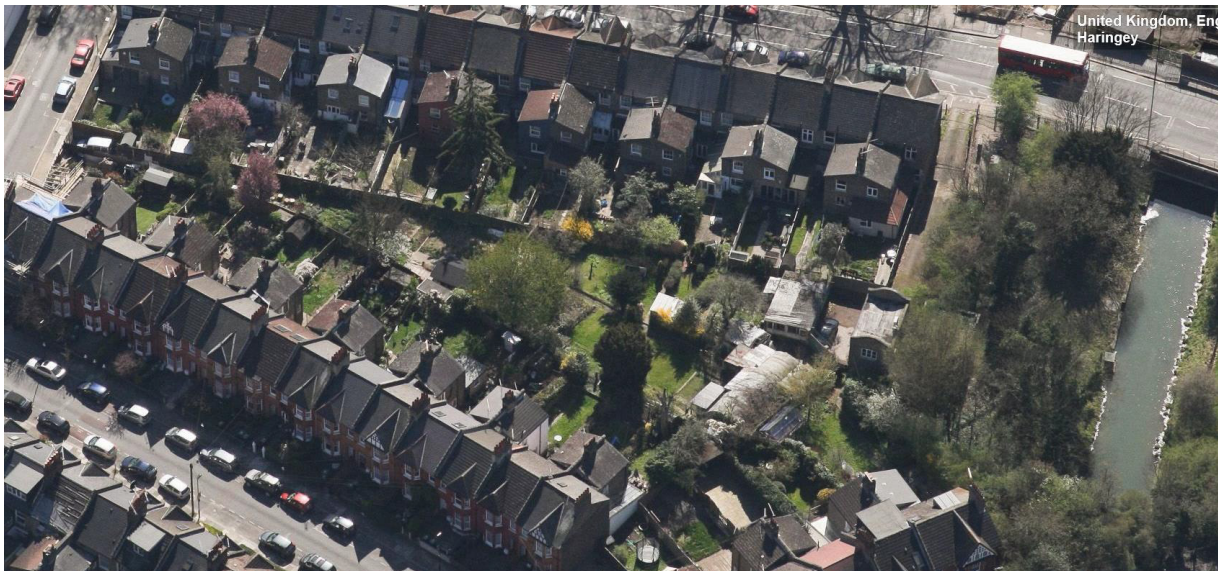




Proposed Site Plan



Site image (looking north)



Site image (looking south)



View of Access Road into site from Station Road.



View of one of existing structures on site



View of one of existing structures on site



Internal view of one of existing structures on site



View of site from the rear of 138 Station Road



View of site from the access from Barratt Avenue



View of site from the access from Barratt Avenue



Basement Plan



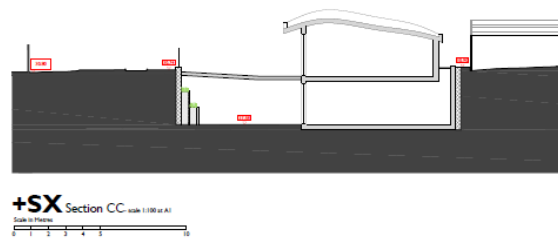
Ground Floor Plan



First Floor Plan



Roof Plan



Section / Elevation Drawing



Side Elevation Drawing of Coach House Building



+SX Section DD - scale 1:100 at A1
Scale in Meters



+SX Section EE - scale 1:100 at A1
Scale in Meters

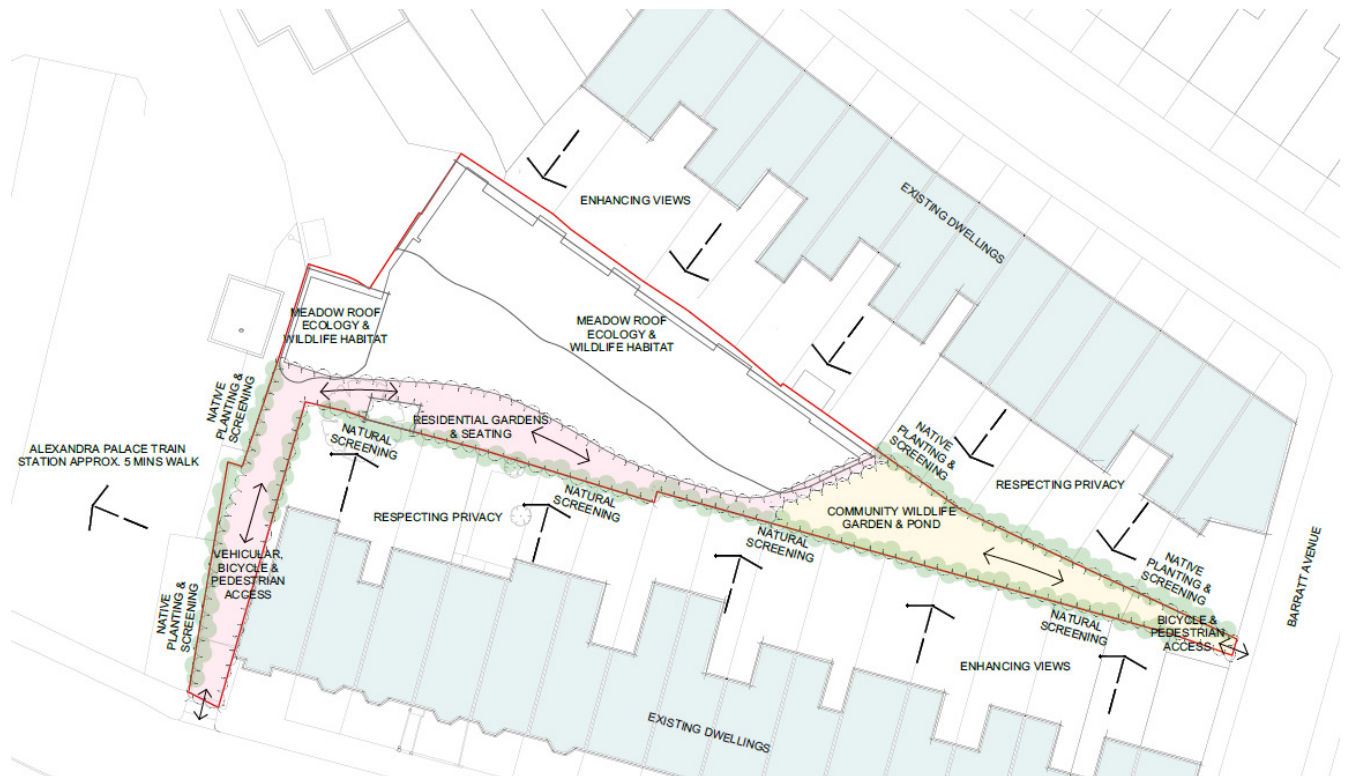


+SX Section FF - scale 1:100 at A1
Scale in Meters

Elevation Drawings



Landscape Plan



Landscape Strategy Plan

Appendix 4 Appeal Decision APP/Y5420/W/18/3196614



Appeal Decision

Site visit made on 13 June 2018

by Elaine Gray MA(Hons) MSc IHBC

an Inspector appointed by the Secretary of State for Communities and Local Government

Decision date: 29th June 2018

Appeal Ref: APP/Y5420/W/18/3196614

Land at the rear of 132 Station Road, Wood Green, London N22 7SX

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
- The appeal is made by Mr Danny Sofizade against the decision of the Council of the London Borough of Haringey.
- The application Ref HGY/2017/2182, dated 21 June 2017, was refused by notice dated 22 January 2018.
- The development proposed is demolition of existing structures and redevelopment of the site to provide 3 no. two storey family sized dwellings (with basement floors) and associated refuse shelters, cycle parking and additional landscaping.

Decision

1. The appeal is dismissed.

Preliminary Matters

2. The description of the development varies between the application form and the subsequent documents. I have used the version given on the decision notice, as it describes the proposal more accurately.

Main Issues

3. The main issues are the effect of the development on the character or appearance of the Wood Green Common Conservation Area, and whether the development would provide adequate living conditions for future occupants, with particular reference to access arrangements to the site.

Reasons

Conservation area

4. The Wood Green Common Conservation Area (CA) is characterised by terraces of residential buildings which are interspersed with large areas of green space that were previously combined as a large common. A number of other building types are present, such as institutional, educational and retail premises. The appeal site comprises a triangular piece of land that is confined on three sides by terraces on Barratt Avenue, Park Avenue and Station Road. These streets comprise predominantly two storey buildings whose traditional character is typical of that found in the CA.
5. The appeal site is currently occupied by a number of structures associated with its use as a garden area for 132 Station Road. It is subservient in terms of the small scale and impermanent nature of the buildings, and its informal, natural

<https://www.gov.uk/planning-inspectorate>

appearance. The historic maps indicate that the site has remained largely undeveloped since it was enclosed by the surrounding development. As a result, its historic form and layout remain clearly legible.

6. The site also provides an important contrast to the bustle of the surrounding streets in a busy urban area. Although largely hidden from public view, it is overlooked by a substantial number of private residences. It is quiet and tranquil, providing a degree of spaciousness within the tight grain of the terraces. The existing planting softens the site's appearance, and provides a buffer between the buildings, promoting an ambiance of privacy and seclusion. I note that the Wood Green Common Conservation Area Appraisal identifies the site as a neutral contributor to the area. However, in my view, all these attributes contribute positively to the significance of the CA.
7. The development would create three detached new dwellings, whose access would be taken from Station Road along the existing lane. The proposed new dwellings would broadly reflect the footprints of the existing structures, and their position closest to the access track. However, the development would introduce a cluster of principal dwellings within an area that is defined by ancillary structures. It therefore would be at odds with the prevailing historic pattern of development in the CA, whereby the principal elevations of the houses face directly onto the street, rather than a secondary space.
8. The new dwellings would be built with a basement level so as to appear single storey in height, and would be installed with green roofs. However, the green roofs would not have the same visual qualities as the garden land. The scheme would also provide landscaped gardens, reflecting to a degree the existing appearance of the site. Nonetheless, by its nature and usage, the residential development would fundamentally alter the character of the site, introducing hardscaping, lighting and domestic paraphernalia where none currently exist. Whilst the development would be physically lower in height than the terraces, the significance of the site as a subordinate space would be lost, and as a result, the character and appearance of the CA would be unacceptably harmed.
9. Accordingly, overall, the proposal conflicts with the overarching statutory duty as set out in the Planning (Listed Buildings and Conservation Areas) Act 1990, which must be given considerable importance and weight, and with the National Planning Policy Framework (the NPPF), which seeks to protect heritage assets. In addition, it would fail to comply with Policy SP11 of the Haringey Local Plan, which requires development to respect local character and historic significance, and Policy DM1 of the Haringey Development Management DPD (DPD), insofar as it seeks a positive contribution to the distinctive character of the local area.
10. It would also conflict with DPD Policy DM9 and Policy 7.8 of the London Plan (LP), which jointly seek to protect heritage assets, LP Policy 7.4, which relates to local character, and LP Policy 7.6, insofar as it requires development design to be appropriate to its context.
11. Although serious, the harm to the heritage asset in this case would be less than substantial, within the meaning of the term in paragraph 133 of the NPPF. Paragraph 134 requires that, where a proposal would lead to less than substantial harm, the harm should be weighed against the public benefits of the proposal.

12. The scheme would add three dwellings to the housing supply in an accessible location, which is to be given significant weight. Were it able to be achieved, the removal of the metal gate would result in a small visual improvement to the CA. However, these factors would not outweigh the harm I have identified.
13. Paragraph 132 of the NPPF states that great weight should be given to the conservation of a designated heritage asset and any harm requires clear and convincing justification. Drawing all the above factors together, the combined public benefits do not outweigh the harm I have identified to the heritage assets.

Living conditions

14. The Council's second reason for refusal relates to the proposed general access arrangements to the site for services. I note that there would be no vehicle access to the development.
15. However, the refuse storage area would be located adjacent to the end of the access track, and I do not consider the distance from the street to be excessive. The Council has suggested a condition be imposed, in the event that I were to allow the appeal, seeking details of a scheme for the storage and collection of refuse from the premises. I am satisfied that such a condition would be sufficient to address this matter. In terms of access for fire-fighting, the installation of sprinkler systems within the new dwellings would be required by Building Regulations to mitigate fire risk, and so the scheme is acceptable in this regard.
16. The track is fairly wide, and is mostly visible from Station Road, and so would benefit from natural surveillance. On my visit, it did not strike me as being unappealing or potentially unsafe, and there is little evidence before me of the security problems alluded to be the Council. I agree that home deliveries are part of modern life, but there is little evidence to suggest that these could not be accommodated.
17. In its statement of case, the Council refers to the issues of access for mobility impaired and elderly users. It is clear that the development would not be suitable for those dependent on the use of the car for mobility. However, it is not unusual in densely populated urban areas for a proportion of residences to be inaccessible directly by car, and car-free developments are common in areas with good public transport links. Therefore, a conflict would arise with DPD Policy DM2, insofar as it requires new developments to be able to be used by all. Nonetheless, having regard to the policy's aims, it would otherwise provide an acceptable standard of access and therefore is not in conflict with the policy as a whole. I therefore find that the conflict in this case would not be so harmful as to warrant the withholding of planning permission.
18. I therefore conclude that the proposed development would provide adequate living conditions for future residents in terms of access arrangements. It would thus accord with DPD Policy DM7, which amongst other things, seeks to ensure good access, and with the overall aims of DPD Policy DM2.

Other Matters

19. The grade II listed tunnel entrance to the New River is located to the west of the site. However, it would not appear to be intervisible with the appeal site,

and there is no compelling evidence to show that its setting would be harmed by the development proposal.

20. I note that the scheme has undergone a number of revisions, and that the planning officer's report was favourable towards the development. I have also had regard to Policy H2 of the Draft London Plan, which relates to small sites. However, these matters have not led me to a different conclusion.
21. My attention has been drawn to four approved developments within the same Council area that the appellant considers comparable to the appeal scheme. All four related to land either within, or on the edge of a conservation area. However, I am not aware of the full details of the circumstances that led to these proposals being accepted, and so I cannot be sure that they represent a direct parallel to the appeal proposal. Whilst these examples demonstrate that the development of small backland sites was acceptable in each of these cases, I am bound to consider the appeal scheme on its own merits, and on the evidence before me.
22. I note that the first two examples, one at land to the rear of Cornwall Avenue and one at the rear of 60-68 Cecile Park, were determined in 2008, and so they pre-date the adoption of the NPPF, which further limits the weight I can afford them. I have also taken into account the recent court judgement, *Dorothy Bohm v SSCLG [2017] EWHC 3217*, which is cited by the appellant. However, I am satisfied that the scheme as a whole has been fully considered in this case.
23. I acknowledge the appellant's stance that the site is a wasted resource, and that the appeal scheme would be an efficient use of the land. However, I do not share the view that open, undeveloped spaces within conservation areas, particularly those within urban locations, are inherently without value. Furthermore, the aim of optimising the development potential of the site would not strike an acceptable balance with the harm I have identified.

Conclusion

24. Despite the fact that the development would provide adequate living conditions for future occupiers, it would unacceptably harm the character and appearance of the conservation area. The proposed development would thus conflict with the development plan as a whole, and so the appeal is dismissed.

Elaine Gray

INSPECTOR

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Report for:	Planning Sub Committee Date: 08 February 2021	Item Number:	
Title:	Update on major proposals		
Report Authorised by:	Dean Hermitage		
Lead Officers:	John McRory & Robbie McNaugher		
Ward(s) affected: All	Report for Key/Non Key Decisions:		

1. Describe the issue under consideration

- 1.1 To advise the Planning Sub Committee of major proposals that are currently in the pipeline. These are divided into those that have recently been approved; those awaiting the issue of the decision notice following a committee resolution; applications that have been submitted and are awaiting determination; and proposals which are the being discussed at the pre-application stage. A list of current appeals is also included.

2. Recommendations

- 2.1 That the report be noted.

3. Background information

- 3.1 As part of the discussions with members in the development of the Planning Protocol 2014 it became clear that members wanted be better informed about proposals for major development. Member engagement in the planning process is encouraged and supported by the National Planning Policy Framework 2019 (NPPF). Haringey is proposing through the new protocol to achieve early member engagement at the pre-application stage through formal briefings on major schemes. The aim of the schedule attached to this report is to provide information



on major proposals so that members are better informed and can seek further information regarding the proposed development as necessary.

4. Local Government (Access to Information) Act 1985

- 4.1 Application details are available to view, print and download free of charge via the Haringey Council website: www.haringey.gov.uk. From the homepage follow the links to 'planning' and 'view planning applications' to find the application search facility. Enter the application reference number or site address to retrieve the case details.
- 4.2 The Development Management Support Team can give further advice and can be contacted on 020 8489 5504, 9.00am-5.00pm Monday to Friday.

Site	Description	Timescales/comments	Case Officer	Manager
APPLICATIONS DETERMINED AWAITING 106 TO BE SIGNED				
Iceland, Land at Brook Road, N22 HGY/2017/2886	Redevelopment of site and erection of four independent residential blocks providing 148 residential units.	Members resolved to grant planning permission subject to the signing of a section 106 legal agreement. Not yet signed but final draft is near completion. Stage 2 agreed with GLA. Finalising final points on S106	Samuel Uff	John McRory
76-84 Mayes Road (former Caxton Road PFS), N22 6TE Caxton Road PFS HGY/2020/0795	Re-development of vacant site to provide a residential led mixed-use development comprising circa 75 C3 units and 1000sqm of commercial floorspace	Members resolved to grant planning permission subject to the signing of a section 106 legal agreement. Legal agreement to be signed.	Tobias Finlayson	John McRory
Former Newstead's Nursing Home, Broadlands Road HGY/2018/3205	Demolition of existing building and erection of three buildings between two and three storeys in heights to provide 10 residential dwellings, private and communal amenity space and other associated development.	Members resolved to grant planning permission subject to the signing of a section 106 legal agreement. Legal agreement to be signed.	Valerie Okeiyi	John McRory
Lockkeepers Cottage, Ferry Lane HGY/2020/0847	Redevelopment of the site comprising the demolition of existing buildings and the erection of a new building ranging in height from 3 to 6 storeys to accommodate 13 residential units (Use Class C3), employment floorspace (Use	Resolution to grant given at July 2020 Committee. Negotiations on the legal agreement are ongoing.	Chris Smith	Robbie McNaugher

	Class B1a) at upper ground and first floor level and retail / café floorspace (Use Class A1 / A3) at lower ground floor level, along with associated landscaping and public realm improvements, cycle parking provision, plant and storage and other associated works.			
Northumberland Terrace 790-814) High Road, Tottenham, N17	THFC proposal for 2,700sqm (GIA) of A1/A2/A3/B1/D1/D2 floorspace and refurbishment of the Listed Buildings fronting the High Road.	Resolution to grant given at Oct 2020 Committee. Negotiations on the legal agreement are ongoing.	Graham Harrington	Robbie McNaugher
Pool Motors 7 Cross Lane N8 HGY/2020/1724	Demolition of existing buildings and erection of two buildings of five storey (Block B) and six storey (Block A) comprising flexible commercial floorspace (Use Class A1, A2, B1(a-c), B8, D1 and D2) at ground floor level of Block A and housing including associated hard and soft landscaping, refuse and recycling storage and car parking and cycle storage.	Members resolved to grant planning permission subject to the signing of a section 106 legal agreement. Legal agreement to be signed.	Valerie Okeiyi	John McRory
APPLICATIONS SUBMITTED TO BE DECIDED				
26-28 Brownlow Road	Demolition of existing buildings; erection of a part-3 and part-4 storey building comprising 23 flats; erection of 1 detached dwelling to the rear with 2 parking spaces, provision of 3 disabled parking spaces at the front; cycle, refuse and recycling storage; provision of new access onto Brownlow Road and accessway to the rear.	Under assessment. Likely to be reported to Members at March planning committee	Tobias Finlayson	John McRory

Unit 7 Unicorn works, 21-25 Garman Road N17 HGY/2020/2576	Reconstruction of the industrial unit which was recently burnt down due to fire that started in a neighbouring industrial building.	<p>Consultation expired. No objections.</p> <p>S106 negotiations underway.</p> <p>Expected to be a delegated decision.</p> <p>Seeking to ensure fencing reflects Mowlem Trading Estate and design coordination with neighbouring sites.</p>	Tania Skelli	Robbie McNaugher
Units 1-6 Unicorn works, 21-25 Garman Road N17 HGY/2020/3186	Reconstruction of the industrial unit (to replace that of a previously destroyed in the fire)	<p>Out to consultation</p> <p>Seeking to ensure fencing reflects Mowlem Trading Estate and design coordination with neighbouring sites.</p>	Tania Skelli	Robbie McNaugher
109 Fortis Green VOID/2020/3396	Demolition of all existing structures and redevelopment of the site to provide 10 residential units (use class C3) comprising of 6 x residential flats and 4 mews houses and 131m2 of flexible retail/ office unit (use class A1/A3/B1) including basement car parking and other associated works.	<p>Invalid</p> <p>Planning permission expired September 2019 with no implementation taking place</p> <p>This scheme is the same scheme approved by the S73 material amendment that was granted in 2017</p>	Roland Sheldon	Matthew Gunning
IN PRE-APPLICATION DISCUSSIONS				
Ashley House (Levenes)	Demolition and rebuild as 20 storey tower for 90 units, with office space	Pre-app meetings held and advice note issued.	Samuel Uff	John McRory

Wood Green Corner Masterplan	Masterplan for Wood Green Corner, as defined in draft Wood Green AAP as WG SA2 (Green Ridings House), SA3 (Wood Green Bus Garage) and SA4 (Station Road Offices)	Pre-app advice issued	Samuel Uff	John McRory
Goods Yard White Hart Lane Banqueting Suite 819-821 High Rd 867-879 High Road	Proposal to amend previous proposals for Goods Yard and 867- 879 High Road and new development on Banqueting Suite site. Part of High Road West Masterplan Area.	Pre-app meetings held and advice note issued.	Graham Harrington	Robbie McNaugher
Broadwater Farm	Demolition and rebuild of Northolt and Tangmere blocks to provide up to 375 homes, landscaping and public realm improvements.	Pre-app meetings and a QRP held. Public consultations ongoing.	Chris Smith	Robbie McNaugher
Mecca Bingo	250-300 residential units, replacement bingo hall and other commercial uses	Pre-app advice note issued.	Chris Smith	John McRory
Mary Fielding Guild Care Home, 103-107 North Hill	Demolition of the existing Mary Feilding Guild Care Home (Use Classes Order C2) and the redevelopment of the site to provide a new 72 bed care home with ancillary communal facilities, services and amenities.	PPA signed. Further pre-app discussions taking place.	Neil McClellan	John McRory
Cranwood House, Muswell Hill Road/Woodside Ave, N10	Redevelopment of site for residential and associated amenity space, landscaping, and parking.	Pre-application discussions ongoing. 2 nd QRP - 26 th Aug 2020. Pre-committee briefing - 11 th March.	Phil Elliott	Robbie McNaugher

		Submission expected Feb 2021		
Remington Road, N15 6SR	Council development of open land and garages for 35 residential units and associated landscaping, public realm improvements, play space, cycling and refuse stores.	Presented as pre-app to Sept Committee QRP on 18th Nov Submission expected Feb 2021	Laurence Ackrill	Robbie McNaugher
Drapers Almshouses Edmansons Close	Amalgamation, extension and adaptation of existing almshouses to provide 22 x 3 bedroom family dwellings; and creation of additional units on site to provide 1 further 3 bedroom dwelling; 7 x 2 bedroom dwellings and 12 x 1 bedroom dwellings (specifically provided for housing for older people)	Pre-application discussions ongoing.	Tobias Finlayson	John McRory
679 Green Lanes	Redevelopment of the site to provide up to 121 new homes, new office and retail space.	Discussions ongoing	Samuel Uff	John McRory
44 Hampstead Lane	Use Class C2 high quality specialist dementia care with 73 en-suite bedrooms and communal facilities	Discussions ongoing	Samuel Uff	John McRory
West Indian Cultural Centre Clarendon Road off Hornsey Park Road	Demolition of the three existing buildings and construction of a part 12/14 and part 6/8/10 storey building incorporating a two storey base to provide a new cultural centre, co-workspace, gym and cafe, 85 residential units and 140 co-living units, with access, public realm improvements and landscaping and car and cycle parking.	Pre-application discussions ongoing.	Tobias Finlayson	John McRory

Selby Centre	Community centre replacement and council housing with improved sports facilities and connectivity	Pre-apps meetings commenced in March. Presented to QRP in May. Talks ongoing with Officers and Enfield Council.	Phil Elliott	Robbie McNaugher
139-143 Crouch Hill	Redevelopment of 139 - 143 Crouch Hill to provide 31 residential units (3 affordable) and 55sqm commercial, with basement parking and additional 250sqm commercial. Maximum height of 6 storeys.	Pre-app meeting held on 22/01/2021. Previously 139-141 but has been extended to include no.143. Pre-app note being drafted.	Samuel Uff	John McRory
573-575 Lordship Lane	Redevelopment of four storey residential development of 17 units.	Pre-app advice notes issued. Third meeting recently held.	Chris Smith	John McRory
48-54 High Road, Wood Green	Redevelopment of the site to create a part 6 storey and part 8 storey mixed use development over the existing retail units at ground floor to provide 76 residential dwellings, 2,800sqm of ground floor retail, 868sqm of first floor retail and office space.	Pre-application letter issued. Revised scheme to be submitted.	Chris Smith	John McRory
25-27 Clarendon Road off Hornsey Park Road	Redevelopment of the site to provide new commercial floorspace, 66 flats over in 9 storey high building with associated parking, and amenity space.	Pre-application response issued.	Valerie Okeiyi	John McRory

Warehouse living proposals: Overbury/Eade Road, Arena Design Centre, Haringey Warehouse District	Warehouse Living and other proposals across 2 sites.	Draft framework presented for Overbury /Eade Road Sites, further pre-application meetings scheduled, PPA signed. QRP 12 February 2020	Phil Elliott	Robbie McNaugher
Warehouse living proposal - Omega Works Haringey Warehouse District	Warehouse Living and other proposals.	Pre-application discussions taking place. QRP 23 Sept 2020	Phil Elliott	Robbie McNaugher
311 Roundway	Mixed Use Redevelopment – 70 Units	Officers have met with one landowner to seek a masterplanned approach. Impact on Bruce Caste will be considered. Pre-application discussions has not yet commenced.	Chris Smith	Robbie McNaugher
High Road West	Comprehensive redevelopment of site for residential led mixed-use scheme	Ongoing pre-application discussions taking place.	Martin Cowie	Robbie McNaugher
Gladstone House, N22	Demolition of existing buildings and erection of 15 storey mixed use commercial and residential for 44 dwellings	Pre-application report issued.	Samuel Uff	John McRory
36-38 Turnpike Lane London	Erection of 9 residential flats and commercial space at ground floor. (major as over 1000 square metres)	Pre-application report issued.	Tania Skelli	John McRory

N8 OPS	(The Demolition of the existing structure and the erection of four-storey building with part commercial/residential on the ground floor and self-contained flats on the upper floors.)			
1 Farrer Mews London N8 8NE	Proposed development to Farrer Mews to replace existing residential, garages & Car workshop into (9 houses & 6 flats)	Second pre-application meeting arranged following revised scheme	Tania Skelli	John McRory
50 Clarendon Road	Retention of existing 3 storey building and internal reconfiguration to create 6 X Commercial units over ground, first and second floor and 8 x residential units over first and second floor	Pre-application discussions taking place Application to be submitted soon	Valerie Okeiyi	John McRory
Osborne Grove Nursing Home/ Stroud Green Clinic 14-16 Upper Tollington Park N4 3EL	Demolition of a 32 bed respite home and clinic building. Erection of a new 70 bed care home and 10 studio rooms for semi-independent living, managed by the care home. Separate independent residential component comprising a mix of twenty self-contained 1 and 2 bedroom flats for older adults, planned on Happi principles. Day Centre for use of residents and the wider community as part of a facility to promote ageing wellness.	Pre-app advice issued No update – nov 20	Tania Skelli	John McRory
Partridge Way, N22	Council development of garages and adjoining land for block of 17 residential units and	Pre-application discussions ongoing. QRP – 18 th March 2020	Conor Guilfoyle	John McRory

	associated landscaping, play space, cycling and refuse stores			
Wat Tyler House, Boyton Road, N8	Council development of car park for block of 14 residential units and associated landscaping, play space, cycling and refuse stores.	First pre-application discussions ongoing discussions	Laurence Ackrill	John McRory
356-358 St. Ann's Road - 40 Brampton Road	Demolition of two buildings on corner of St. Ann's Rd and of coach house and end of terrace home on Brampton Rd and replacement with increased commercial and 9 self-contained homes.	Pre-application meeting held 30/07.	Phil Elliott	Robbie McNaugher
29-33 The Hale	'Shoulder' of 7 storeys and a 23-storey tower. Commercial at ground floor with residential above. Residential would comprise 366 co-living rooms or 435 rooms of student accommodation.	Pre-application meeting to be held 19/08. PPA agreed.	Phil Elliott	Robbie McNaugher
Highgate Lodge 9 Waverley Road N8 9QS	Demolition of property behind retained façade to provide a new 'co-living' scheme comprising 44 co-living rooms and associated facilities.	Pre-app advice issued	Tania Skelli	John McRory
Branksome Courtenay Avenue London N6 4LP	Demolition of existing detached dwelling house incorporating ground, first and partial second floor levels, garage, ancillary pool building and link structure and provision of replacement detached dwelling house incorporating basement, ground, first and second floor levels	PPA discussions ongoing	Tania Skelli	John McRory

399-401 High Road, N17 (Formerly known as Chances)	Reordering and extension of no.399/401 High Road to form a school.	Pre-app advice issued Change of use application to be submitted with Listed Building works.	Valerie Okeiyi	Robbie McNaugher
(Part Site Allocation SA49) Lynton Road London, N8 8SL	Demolition/Part Demolition of existing commercial buildings and mixed use redevelopment to provide 75 apartments and retained office space	Pre-app to be issued	Tobias Finlayson	John McRory
Cross House, 7 Cross Lane	Demolition of existing building and replacement with new build building consisting of 810sqm commercial space and 9 apartments.	Pre-app advice issued and discussions ongoing	Valerie Okeiyi	John McRory
Brunel Walk and Turner Avenue	Council development - Preliminary meeting to discuss matters of principle in relation to the siting, scale, massing of the proposed new development on Brunel Walk (c. 45 units) and the associated and comprehensive improvement/reconfiguration of the public realm/landscaping treatment on the Turner Avenue Estate.	Pre-app meeting held 17/12/20 Pre-app advice to be issued	Valerie Okeiyi	Robbie McNaugher
Chocolate Factory	Minor changes to approved block E (S96a) Changes to block B (S73) Changes to wording of some conditions (S96a) Changes to S106 (Deed of variation)	Chocolate factory review meetings with applicant (Workspace) on-going	Valerie Okeiyi	John McRory

Braemar Avenue Baptist Church, Braemar Avenue.	Demolition of dilapidated church hall, to allow construction of part 3, part 4 storey building (over basement) comprising new church hall extensions (204m2) and 16 flats. Internal and minor external alterations to adjacent listed church, together with landscaping improvements	Pre-application discussions taking place	Valerie Okeiyi	John McRory
157-159 Hornsey Park Road, Wood Green	Redevelopment of existing dilapidated construction yard to provide 40 new-build self-contained flats.	Pre-app advice issued.	Valerie Okeiyi	John McRory
PRE/2020/0246 – Far Field Sports Ground, Courtenay Avenue.	Various re-surfacing works to field and associated infrastructure	Pre-app note written up awaiting issue	Laurence Ackrill	John McRory
Major Application Appeals				
Guildens, Courtenay Avenue	Demolition of existing dwelling with retention of front facade and erection of replacement two-storey dwelling and associated extension to lower ground floor and the creation of a basement level.	Appeal submitted. Written representations. Decision expected soon.	Laurence Ackrill Manager: John McRory	
300-306 West Green Road HGY/2020/0158	Demolition of existing buildings and erection of a five-storey building (plus basement) comprising of a retail unit at ground and basement levels and nineteen residential units above; and associated landscaping and the provision of an outdoor children's play area	Appeal submitted for Written Representations procedure. No start date confirmed by the Inspectorate.	Chris Smith Manager: Robbie McNaugher	

10 Gourley Street HGY/2020/1183	1000sqm+ of new office and warehouse space.	Appeal procedure changed to a Virtual Hearing and process re-started. Council Appeal Statement has now been submitted. Hearing date delayed by the Inspectorate.	Chris Smith Manager: Robbie McNaugher
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HARINGEY COUNCIL

PLANNING COMMITTEE

APPLICATIONS DECIDED UNDER DELEGATED POWERS BETWEEN 28/12/2020 AND 22/01/2021

BACKGROUND PAPERS

For the purpose of the Local Government (Access to Information) Act 1985, the background papers in respect of the following items comprise the planning application case file.

In addition application case files are available to view print and download free of charge via the Haringey Council website:
www.haringey.gov.uk

From the homepage follow the links to 'planning' and 'view planning applications' to find the application search facility.
Enter the application reference number or site address to retrieve the case details.

The Development Management Support Team can give further advice and can be contacted on 020 8489 5504,
9.00am - 5.00pm, Monday - Friday.

Please see Application type codes below which have been added for your information within each Ward:

Application Type codes:

ADV	Advertisement Consent
CAC	Conservation Area Consent
CLDE	Certificate of Lawfulness (Existing)
CLUP	Certificate of Lawfulness (Proposed)
COND	Variation of Condition
EXTP	Replace an Extant Planning Permission
FUL	Full Planning Permission
FULM	Full Planning Permission (Major)
LBC	Listed Building Consent
LCD	Councils Own Development
LCDM	(Major) Councils Own Development
NON	Non-Material Amendments
OBS	Observations to Other Borough
OUT	Outline Planning Permission
OUTM	Outline Planning Permission (Major)
REN	Renewal of Time Limited Permission
RES	Approval of Details
TEL	Telecom Development under GDO
TPO	Tree Preservation Order application works

Recommendation Type codes:

GTD	Grant permission
REF	Refuse permission
NOT DEV	Permission not required - Not Development
PERM DEV	Permission not required - Permitted
PERM REQ	Development
RNO	Permission required
ROB	Raise No Objection

WARD: **Alexandra****CLUP Applications Decided: 1**

Application No: **HGY/2021/0033** Officer: Janey Zhao
 Decision: PERM DEV Decision Date: 22/01/2021
 Location: 10 Grasmere Road N10 2DJ
 Proposal: Certificate of lawfulness: proposed demolition of rear conservatory and insertion of one ground floor side elevation window and replacement of front and rear window frames.

FUL Applications Decided: 8

Application No: **HGY/2020/2774** Officer: Laina Levassor
 Decision: REF Decision Date: 07/01/2021
 Location: 98 Palace Gates Road N22 7BL
 Proposal: Formation of roof terrace and associated screening.

Application No: **HGY/2020/2790** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 22/01/2021
 Location: 374 Alexandra Park Road N22 7BD
 Proposal: Ground and lower ground floor rear extension to facilitate the conversion of single family dwellinghouse into 4 x self contained flats with associated car and cycle parking; refuse/recycle storage and landscaping of front garden area.

Application No: **HGY/2020/2813** Officer: Matthew Gunning
 Decision: GTD Decision Date: 22/01/2021
 Location: 93 The Avenue N10 2QG
 Proposal: Redesign of the rear part of the property to create a new kitchen/living space, including new extension for patio area at rear of property and replacement of windows and doors.

Application No: **HGY/2020/2937** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 07/01/2021
 Location: 5 Kendalmer Close N10 2DF
 Proposal: Installation of roof light in dwelling house.

Application No: **HGY/2020/2982** Officer: Laurence Ackrill
 Decision: REF Decision Date: 18/01/2021
 Location: Garages rear of 75 Windermere Road N10 2RD
 Proposal: Demolition of existing 5no garages and erection of 1no 3 bedroom single storey house.

Application No: **HGY/2020/3021** Officer: Roland Sheldon
 Decision: GTD Decision Date: 11/01/2021
 Location: 44 Alexandra Park Road N10 2AD
 Proposal: Erection of rear dormer with associated staircase and railings to facilitate creation of roof terrace.

Application No: **HGY/2020/3045** Officer: Matthew Gunning
 Decision: GTD Decision Date: 14/01/2021
 Location: Flat A 36 Muswell Avenue N10 2EG
 Proposal: Proposed outbuilding in the rear garden, rear facade alteration, internal floor plan redesign and all associated works at 36A Muswell Avenue.

Application No: **HGY/2020/3189** Officer: Samuel Uff
 Decision: GTD Decision Date: 13/01/2021
 Location: 125 Crescent Road N22 7RU
 Proposal: Erection of rear roof extensions to outrigger and main roof and installation of front rooflights

NON Applications Decided: 1

Application No: **HGY/2021/0231** Officer: Samuel Uff
 Decision: GTD Decision Date: 22/01/2021
 Location: 11 Winton Avenue N11 2AS
 Proposal: Non-material amendment to the roof extension approved under HGY/2020/0703 to alter the rear dormer glazing and front rooflights

RES Applications Decided: 2

Application No: **HGY/2020/3227** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 05/01/2021
 Location: First Floor Flat 2 214 Victoria Road N22 7XQ
 Proposal: Approval of details reserved by condition 4 attached to planning permission HGY/2020/0880.

Application No: **HGY/2021/0032** Officer: Roland Sheldon
 Decision: GTD Decision Date: 11/01/2021
 Location: 105 Rosebery Road N10 2LD
 Proposal: Discharge of condition 5 (details of suitably qualified engineer) of planning application HGY/2020/2674.

TPO Applications Decided: 1

Application No: **HGY/2020/2612** Officer: Janey Zhao
 Decision: GTD Decision Date: 19/01/2021
 Location: 32 Barnard Hill N10 2HB
 Proposal: Works to trees protected by a TPO. Fell and treat TG1 Oak trees

Reason: To remedy the differential foundation movement at the insured property and to ensure the long-term stability of the building. It is the case that an alternative to felling such as pruning or significant 'pollarding' of the tree would not provide a reliable or sustainable remedy to the subsidence. Insurers understanding the requirement to offer replacement planting in the event consent to fell is granted.

Total Applications Decided for Ward: 13

WARD: **Bounds Green**

CLUP Applications Decided: 3

Application No: **HGY/2020/2977** Officer: Laina Levassor
 Decision: PERM DEV Decision Date: 11/01/2021
 Location: 26 Durnsford Road N11 2EH
 Proposal: Certificate of Lawfulness for proposed hip to gable and rear dormer extensions to facilitate loft conversion.

Application No: **HGY/2020/3098** Officer: Mercy Oruwari
 Decision: PERM DEV Decision Date: 18/01/2021
 Location: 4 Cameron Close N22 8EY
 Proposal: Certificate of lawfulness for the erection of a single storey rear extension.

Application No: **HGY/2021/0002** Officer: Emily Whittredge
 Decision: PERM DEV Decision Date: 06/01/2021
 Location: 26 Gordon Road N11 2PB
 Proposal: Certificate of lawfulness: proposed hip to gable and rear dormer loft conversion

FUL Applications Decided: 5

Application No: **HGY/2020/2850** Officer: Tania Skelli
 Decision: GTD Decision Date: 15/01/2021
 Location: Land R/O Electricity sub-station adjoining 2 Lascotts Road N22
 Proposal: Erection of a 2 storey building to create 2no. self-contained flats with associated cycle parking and refuse storage.

Application No: **HGY/2020/2894** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 11/01/2021
 Location: 7 Bounds Green Court Bounds Green Road N11 2EX
 Proposal: Retrospective application for change of use of property from residential flat (C3) to house in multiple occupation (HMO)(C4) for up to 4 occupants.

Application No: **HGY/2020/3022** Officer: Sarah Madondo
 Decision: REF Decision Date: 05/01/2021
 Location: 18 Truro Road N22 8EL
 Proposal: Retrospective planning application for the election of a ground floor rear extension and its use as a separate self-contained flat.

Application No: **HGY/2020/3109** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 18/01/2021
 Location: Prince of Wales 1 Finsbury Road N22 8PA
 Proposal: Installation of two awnings on eastern elevation.

Application No: **HGY/2020/3114** Officer: Tania Skelli
 Decision: GTD Decision Date: 19/01/2021
 Location: 49 Blake Road N11 2AG
 Proposal: Erection of two storey side extension and single storey rear extension.

OBS Applications Decided: 1

Application No: **HGY/2020/2958** Officer: Tania Skelli
 Decision: NOT DET Decision Date: 18/01/2021
 Location: Land R/O Electricity sub-station adjoining 2 Lascotts Road N22
 Proposal: Erection of a 2 storey building to create 2no. self-contained flats with associated cycle parking and refuse storage (Joint application with Haringey Council, Ref: HGY/2020/2850). (Observations to L.B. Enfield - their reference 20/03613/FUL)

PNE Applications Decided: 1

Application No: **HGY/2020/2979** Officer: Laina Levassor
 Decision: PN NOT REQ Decision Date: 05/01/2021
 Location: 399 High Road N22 8JB
 Proposal: Erection of a single storey extension which extends beyond the rear wall of the original house by 6m, for which the maximum height would be 3m and for which the height of the eaves would be 2.7m

TEL Applications Decided: 1

Application No: **HGY/2020/3008** Officer: Kwaku Bossman-Gyamera
 Decision: PN REFUSED Decision Date: 06/01/2021
 Location: Area of grass located opposite the Ambulance Station Bounds Green Road N22 8DF
 Proposal: Proposed 5G Telecoms Installation of 18m high street pole c/w wrap-around cabinet and 3no. cabinets with ancillary works to be located on a grassed area off Bounds Green Road/ Trinity Road, opposite the ambulance station, Haringey. (Prior notification: Development by telecoms operators).
 (Prior notification: Development by telecoms operators).

Total Applications Decided for Ward: 11WARD: **Bruce Grove****ADV Applications Decided: 1**

Application No: **HGY/2019/2370** Officer: Neil McClellan
 Decision: GTD Decision Date: 04/01/2021
 Location: 491 High Road N17 6QA
 Proposal: Advertisement consent for new shopfront signage comprising a new painted fascia panel with new illuminated lozenge; new illuminated projecting sign and new illuminated ATM surround.

CLUP Applications Decided: 1

Application No: **HGY/2020/2994** Officer: Emily Whittredge
 Decision: PERM DEV Decision Date: 04/01/2021
 Location: 9 Ranelagh Road N17 6XY
 Proposal: Certificate of Lawfulness for: installation of rear roof light.

FUL Applications Decided: 5

Application No: **HGY/2019/2365** Officer: Neil McClellan
 Decision: GTD Decision Date: 04/01/2021
 Location: 491 High Road N17 6QA
 Proposal: Repair and alteration of existing shopfront including the repair and painting of the door and window frames; the repair and painting of the pilasters and entrance recess; and the installation of new stall riser tiles.

Application No:	HGY/2020/2780	Officer:	Kwaku Bossman-Gyamera
Decision:	GTD	Decision Date:	05/01/2021
Location:	27 Woodside Gardens N17 6UY		
Proposal:	Single storey side extension to form granny annexe with front garden.		
Application No:	HGY/2020/2890	Officer:	Sarah Madondo
Decision:	GTD	Decision Date:	19/01/2021
Location:	84 Bruce Grove N17 6UZ		
Proposal:	Ground floor rear extension to retail shop.		
Application No:	HGY/2020/3050	Officer:	Gareth Prosser
Decision:	GTD	Decision Date:	04/01/2021
Location:	Flat B 318 Mount Pleasant Road N17 6HA		
Proposal:	Conversion of the existing first floor flat in to two separate self-contained flats including the conversion of the loft, a dormer extension to the rear and the insertion of three roof lights to the front (re submission of the existing planning permission HGY/2018/0466).		
Application No:	HGY/2020/3144	Officer:	Sarah Madondo
Decision:	GTD	Decision Date:	19/01/2021
Location:	118 Arnold Road N15 4JH		
Proposal:	Erection of a single storey wrap around rear extension.		

PNE Applications Decided: 1

Application No:	HGY/2020/3033	Officer:	Laina Levassor
Decision:	PN REFUSED	Decision Date:	06/01/2021
Location:	126 Higham Road N17 6NR		
Proposal:	Erection of a single storey extension which extends beyond the rear wall of the original house by 5m, for which the maximum height would be 3.4m and for which the height of the eaves would be 3m		

RES Applications Decided: 6

Application No:	HGY/2019/0761	Officer:	Tobias Finlayson
Decision:	REF	Decision Date:	12/01/2021
Location:	5 Bruce Grove N17 6RA		
Proposal:	Approval of details pursuant to condition 3 (external materials) attached to planning permission HGY/2014/1041.		
Application No:	HGY/2019/0764	Officer:	Tobias Finlayson
Decision:	REF	Decision Date:	12/01/2021
Location:	5 Bruce Grove N17 6RA		
Proposal:	Approval of details pursuant to condition 6a (Risk Assessment - demolition) attached to planning permission HGY/2014/1041.		
Application No:	HGY/2019/0765	Officer:	Tobias Finlayson
Decision:	REF	Decision Date:	12/01/2021
Location:	5 Bruce Grove N17 6RA		
Proposal:	Approval of details pursuant to condition 6b (Risk Assessment - construction) attached to planning permission HGY/2014/1041.		

Application No:	HGY/2019/0767	Officer:	Tobias Finlayson
Decision:	REF	Decision Date:	12/01/2021
Location:	5 Bruce Grove N17 6RA		
Proposal:	Approval of details pursuant to condition 10 (Construction Management Plan (CMP) and Construction Logistics Plan (CLP)) attached to planning permission HGY/2014/1041.		
Application No:	HGY/2020/2827	Officer:	Gareth Prosser
Decision:	GTD	Decision Date:	14/01/2021
Location:	Construction House 579C High Road N17 6SB		
Proposal:	Approval of details pursuant to Condition 8 (Pollution) attached to applicaiton HGY/2017/0670.		
Application No:	HGY/2020/2881	Officer:	Kwaku Bossman-Gyamera
Decision:	GTD	Decision Date:	06/01/2021
Location:	479-481 High Road N17 6QA		
Proposal:	Approval of details pursuant to Condition 3 (Method of Construction Statement), Condition 6 (Details of type and location of secure/covered cycle parking facilities), Condition 7 (Details of storage/collection of refuse), and Condition 8 (Detail Travel Plan) attached to planning permission HGY/2018/2391.		

Total Applications Decided for Ward: 14

WARD: Crouch End

FUL Applications Decided: 2

Application No:	HGY/2020/3051	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	04/01/2021
Location:	Ground Floor Flat 1 7 Aubrey Road N8 9HH		
Proposal:	Construction of a single storey rear extension.		
Application No:	HGY/2020/3084	Officer:	Conor Guilfoyle
Decision:	GTD	Decision Date:	19/01/2021
Location:	First Floor Flat C 79 Ferme Park Road N8 9SA		
Proposal:	Formation of hip-to-gable roof extension; insertion of front roof light; erection of rear dormer roof extension; formation of rear roof terrace.		

TEL Applications Decided: 1

Application No:	HGY/2020/3282	Officer:	Kwaku Bossman-Gyamera
Decision:	PERM DEV	Decision Date:	08/01/2021
Location:	Avenue Heights 3-5 Avenue Road N6 5DS		
Proposal:	formal notification in writing of 28 days notice in advance, in accordance with Regulation 5 of the Electronic Communications Code (Conditions and Restrictions) Regulations 2003 (as amended).		
	The proposed development comprises of the replacement of 3no. antennas with 3no. new antennas and ancillary works thereto.		

TPO Applications Decided: 1

Application No:	HGY/2020/3072	Officer:	Janey Zhao
Decision:	GTD	Decision Date:	08/01/2021
Location:	34 Stanhope Road N6 5NG		

Proposal: Works to tree protected by a TPO: T1- Central Horse Chestnut tree- Pollard to a height of approximately 4.5 metres (just below branch tear). Remove Ivy.

Reason: The tree has recently shed a large branch. There is a long section of bark missing at the back of the trunk and a lot of the bark is cracked. The tree needs to be severely reduced on safety grounds.

Total Applications Decided for Ward: 4

WARD: Fortis Green

CLUP Applications Decided: 1

Application No: **HGY/2020/3124** Officer: Roland Sheldon
 Decision: PERM DEV Decision Date: 22/01/2021
 Location: 1 The Terrace Lauradale Road N2 9LX
 Proposal: Certificate of Lawfulness for the erection of rear dormer and insertion of 4 front rooflights.

FUL Applications Decided: 7

Application No: **HGY/2020/2864** Officer: Roland Sheldon
 Decision: GTD Decision Date: 04/01/2021
 Location: 20A Queens Avenue N10 3NR
 Proposal: Removal of existing shed and erection of replacement garden shed.

Application No: **HGY/2020/2887** Officer: Laurence Ackrill
 Decision: REF Decision Date: 05/01/2021
 Location: R/O Tetherdown Hall 1A Tetherdown N10 1ND
 Proposal: Conversion of 3 No. garages into 2 x No. 1 bed 1 person dwellings involving the insertion of rooflights.

Application No: **HGY/2020/2910** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 15/01/2021
 Location: 17 Marriott Road N10 1JJ
 Proposal: Erection of a rear dormer and hip to gable extension.

Application No: **HGY/2020/2923** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 06/01/2021
 Location: 69 Creighton Avenue N10 1NR
 Proposal: Construction of a ground floor garage side extension, first floor side extension, rear ground floor infill extension, hip to gable roof extension with a dormer and 3no. skylights to front and metal gate and fence, stairs and entrance porch.

Application No: **HGY/2020/2973** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 04/01/2021
 Location: 59 Creighton Avenue N10 1NR
 Proposal: Construction of a single storey rear extension.

Application No: **HGY/2020/2976** Officer: Roland Sheldon
 Decision: GTD Decision Date: 04/01/2021
 Location: 4 The Drive N6 4TD
 Proposal: Erection of single storey ground floor rear extension.

Application No: **HGY/2020/3067** Officer: Roland Sheldon
 Decision: GTD Decision Date: 11/01/2021
 Location: 17 Coldfall Avenue N10 1HS
 Proposal: Erection of single storey ground floor rear extension (revised description).

NON Applications Decided: 1

Application No: **HGY/2021/0068** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 14/01/2021
 Location: Muswell Hill Synagogue Tetherdown N10 1ND
 Proposal: Non material amendment to planning permission reference HGY/2019/3095 for refurbishment works fully outlined in supporting statement, including minor revision to the design of the new security room window and secondary entrance cladding; enlarged glazing to the main entrance screen; additional door from small prayer room into the rear car park; omission of a window in the east elevation; omission of a small extensions to the rear and the storage space within the car park; and additional ventilation plant required on the first floor roof and main roof.

PNE Applications Decided: 1

Application No: **HGY/2020/3137** Officer: Laina Levassor
 Decision: PN NOT REQ Decision Date: 14/01/2021
 Location: 32 Steeds Road N10 1JD
 Proposal: Erection of single storey extension which extends beyond the rear wall of the original house by 6m, for which the maximum height would be 3.4m and for which the height of the eaves would be 2.8m

RES Applications Decided: 1

Application No: **HGY/2019/1604** Officer: Valerie Okeiyi
 Decision: NPW Decision Date: 07/01/2021
 Location: 109 Fortis Green N2 9HR
 Proposal: Approval of details pursuant to condition 20 (extract fans / flues) attached to planning permission HGY/2017/0432

TPO Applications Decided: 1

Application No: **HGY/2020/3070** Officer: Janey Zhao
 Decision: GTD Decision Date: 11/01/2021
 Location: 35 Fordington Road N6 4TD
 Proposal: Works to tree protected by a TPO:

T1 Oak (front of property):
 -Remove all deadwood, basal and epicormic growth to crown break point.
 -Remove diseased and broken branches 2 cm in diameter and larger throughout crown to improve health and appearance and reduce risk of branch failure
 -Reduce lateral crown spread by approximately 1.5m to previous and outermost pruning points on north, east and south sides, including cutting from streetlight to give 1.5m clearance.

Total Applications Decided for Ward: 12WARD: **Harringay****CLDE Applications Decided: 2**

Application No: **HGY/2020/3056** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 11/01/2021
 Location: 43-45 Turnpike Lane N8 0EP
 Proposal: Certificate of lawfulness for the existing use of the property as six self-contained flats.

Application No: **HGY/2021/0006** Officer: Laina Levassor
 Decision: REF Decision Date: 13/01/2021
 Location: 104 Wightman Road N4 1RN
 Proposal: Certificate of Lawfulness for the existing use of 104 Wightman Road as seven self-contained flats

CLUP Applications Decided: 1

Application No: **HGY/2020/3306** Officer: Laina Levassor
 Decision: PERM DEV Decision Date: 12/01/2021
 Location: 48 Park Road N15 3HR
 Proposal: Certificate of Lawfulness for proposed rear and side dormer extensions to facilitate loft conversion.

FUL Applications Decided: 6

Application No: **HGY/2020/2902** Officer: Samuel Uff
 Decision: GTD Decision Date: 04/01/2021
 Location: 108 Allison Road N8 0AS
 Proposal: Conversion of dwelling to 2 x self-contained flats in conjunction with the erection of a single storey rear extension.

Application No: **HGY/2020/2949** Officer: Tobias Finlayson
 Decision: GTD Decision Date: 04/01/2021
 Location: First Floor Flat B 89 Mattison Road N4 1BQ
 Proposal: Formation of roof terrace with access from existing dormer.

Application No: **HGY/2020/3037** Officer: Roland Sheldon
 Decision: REF Decision Date: 08/01/2021
 Location: 60 Wightman Road N4 1RU
 Proposal: Retention of existing single storey side extension, change of use of extension to A5 Use Class Takeaway including external ducting, raised decking to the front entrance.

Application No: **HGY/2020/3040** Officer: Matthew Gunning
 Decision: REF Decision Date: 04/01/2021
 Location: 106A Fairfax Road N8 0NL
 Proposal: Change of use from C3 (dwellinghouses) to C4 (houses in multiple occupation).

Application No: **HGY/2020/3044** Officer: Matthew Gunning
 Decision: GTD Decision Date: 19/01/2021
 Location: 117 Wightman Road N4 1RJ
 Proposal: Single storey rear and side extension.

Application No: **HGY/2020/3061** Officer: Roland Sheldon
 Decision: GTD Decision Date: 13/01/2021
 Location: 23 Burgoyne Road N4 1AA
 Proposal: Erection of single storey side ground floor infill extension.

NON Applications Decided: 1

Application No: **HGY/2020/3224** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 04/01/2021
 Location: Garage Colony rear of Mountview Court St Margarets Avenue N15 3DH
 Proposal: Non material amendment to vary the 'trigger point' of the requirements of Condition 12 attached to planning permission HGY/2020/0181 from pre-commencement to pre-occupation.

RES Applications Decided: 4

Application No: **HGY/2020/3005** Officer: Roland Sheldon
 Decision: GTD Decision Date: 05/01/2021
 Location: Garage Colony rear of Mountview Court St Margarets Avenue N15
 Proposal: Part discharge of condition 13 (part C method statement) of planning permission ref. HGY/2020/0181 granted on 13/3/2020.

Application No: **HGY/2020/3145** Officer: Sarah Madondo
 Decision: GTD Decision Date: 19/01/2021
 Location: 69 Sydney Road N8 0ET
 Proposal: Approval of details reserved by a condition 3 (Refuse and waste storage) attached to HGY/2019/3079.

Application No: **HGY/2020/3146** Officer: Sarah Madondo
 Decision: GTD Decision Date: 15/01/2021
 Location: 69 Sydney Road N8 0ET
 Proposal: Approval of details reserved by a condition 4 (Secure cycle parking) attached to HGY/2019/3079

Application No: **HGY/2020/3196** Officer: Roland Sheldon
 Decision: GTD Decision Date: 21/01/2021
 Location: Garage Colony rear of Mountview Court St Margarets Avenue N15
 Proposal: Discharge of condition 15 (Secured By Design) of planning permission reference HGY/2020/0181 for Demolition of existing garages off St Margarets Avenue. Erection of 4 x 4 Bed, 6-person dwellings (3 storeys).

Total Applications Decided for Ward: 14**WARD: Highgate****CLUP Applications Decided: 1**

Application No: **HGY/2021/0026** Officer: Tobias Finlayson
 Decision: PERM DEV Decision Date: 21/01/2021
 Location: 4 Highgate Close N6 4SD
 Proposal: Certificate of lawfulness (proposed use) for conversion of garage into kitchen, including bricking up of garage doorway and insertion of new window - all in keeping with the works carried out by adjacent neighbours on both sides.

FUL Applications Decided: 8

Application No:	HGY/2020/2859	Officer:	Conor Guilfoyle
Decision:	REF	Decision Date:	04/01/2021
Location:	Bloomfield Court Bloomfield Road N6 4ES		
Proposal:	Creation of additional third floor roof extension to accommodate 4 x 1 bedroom flats within; with associated external alterations to rear elevation glazing and re-covering of retained elements of existing roof.		
Application No:	HGY/2020/2975	Officer:	Roland Sheldon
Decision:	GTD	Decision Date:	04/01/2021
Location:	9 Willowdene 18 View Road N6 4DE		
Proposal:	Single storey ground floor rear and front infill extensions, alterations to rear fenestration.		
Application No:	HGY/2020/2980	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	12/01/2021
Location:	Highgate Pre Preparatory School Bishopswood Road N6 4PP		
Proposal:	Extension to Highgate School Pre-Preparatory building to provide three additional classrooms, a library, covered outdoor play space and level access to the existing school building.		
Application No:	HGY/2020/3041	Officer:	Matthew Gunning
Decision:	GTD	Decision Date:	06/01/2021
Location:	1 Highgate Avenue N6 5RX		
Proposal:	Use of property as single dwelling house		
Application No:	HGY/2020/3043	Officer:	Matthew Gunning
Decision:	GTD	Decision Date:	06/01/2021
Location:	3 Highgate Avenue N6 5RX		
Proposal:	Use of property as single dwelling house.		
Application No:	HGY/2020/3129	Officer:	Gareth Prosser
Decision:	REF	Decision Date:	05/01/2021
Location:	41 Wood Lane N6 5UD		
Proposal:	Erection of front, side, and rear dormer windows.		
Application No:	HGY/2020/3212	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	18/01/2021
Location:	Flat 2 252 Archway Road N6 5AX		
Proposal:	Creation of a balcony area at the rear of the second floor flat.		
Application No:	HGY/2020/3214	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	22/01/2021
Location:	252 Archway Road N6 5AX		
Proposal:	Alterations to roofline to create a sunken balcony area.		

Application No: **HGY/2019/2944** Officer: Gareth Prosser
 Decision: GTD Decision Date: 15/01/2021
 Location: Oakleigh 42 Hampstead Lane N6 4LL
 Proposal: Demolition of existing house and erection of replacement dwelling and associated works.

NON Applications Decided: 1

Application No: **HGY/2020/3267** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 05/01/2021
 Location: 2 Highgate Avenue N6 5RX
 Proposal: Non-material amendment following a grant of planning permission HGY/2020/1881 involving amendments to wording of Condition 9 (Tree protection measures).

RES Applications Decided: 2

Application No: **HGY/2020/2943** Officer: Tania Skelli
 Decision: GTD Decision Date: 04/01/2021
 Location: Jacksons Lane Community Centre 269A Archway Road N6 5AA
 Proposal: Discharge of details pursuant to condition 5 (Tree Protection Method Statement) of planning permission ref. HGY/2019/0462 granted on 29/7/2019 for the alterations including re-opening old church porch as main entrance, new ramp to south-western side of porch, new two-storey roof to replace existing over existing entrance foyer, alterations to Archway Road stairs, new plant and internal alterations to improve accessibility and circulation, installation of lift, new WC provision, and new internal staircase, in association with existing community centre.

Application No: **HGY/2020/3226** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 05/01/2021
 Location: 63 Talbot Road N6 4QX
 Proposal: Approval of details pursuant to condition 4 (Bin storage), 5 (Cycle storage) attached to planning permission HGY/2018/0502.

TPO Applications Decided: 2

Application No: **HGY/2020/2987** Officer: Janey Zhao
 Decision: GTD Decision Date: 11/01/2021
 Location: 2 Bishopswood Road N6 4PR
 Proposal: Works to tree protected by a TPO: T2: Ash (16m): Reduce lowest branch on north side to upright growing point between the fungal bracket and the main trunk leaving approximately 1 metre of branch. (works to T1 Yew will be considered separately under a Section 211 Notice)

Application No: **HGY/2020/3077** Officer: Janey Zhao
 Decision: GTD Decision Date: 05/01/2021
 Location: Highgate Golf Club Denewood Road N6 4AH
 Proposal: Works to tree protected by a TPO.
 Edge of golf course T1 Balsam Poplar x 1 (Populus balsamifera)
 Reduce back to previous reduction points, removing up to 7m of growth, leaving some furnishing growth back at the trunk.

Total Applications Decided for Ward: 15WARD: **Hornsey****FUL Applications Decided: 4**

Application No:	HGY/2020/0590	Officer:	Roland Sheldon
Decision:	GTD	Decision Date:	06/01/2021
Location:	Car Dealership 23-23A High Street N8 7QB		
Proposal:	Demolition of existing building and construction of a mixed-use scheme at part two and part three storeys in height (with accommodation in the roofspace) providing retail floorspace (Class A1) at ground floor level, together with 7 residential flats (Class C3) and 2 residential houses (Class C3), with associated cycle parking, landscaping and ancillary works.		
Application No:	HGY/2020/2812	Officer:	Matthew Gunning
Decision:	GTD	Decision Date:	15/01/2021
Location:	51 Glebe Road N8 7DA		
Proposal:	Rearrangement of front garden including enlargement of existing light well and removal of 2 trees.		
Application No:	HGY/2020/2915	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	04/01/2021
Location:	Flat 1 Bank Chambers 120 High Street N8 7NN		
Proposal:	Installation of Conservation rooflight within rear roof slope.		
Application No:	HGY/2020/3192	Officer:	Laurence Ackrill
Decision:	GTD	Decision Date:	15/01/2021
Location:	43 Harvey Road N8 9PD		
Proposal:	Construction of a single storey rear extension.		

PNE Applications Decided: 2

Application No:	HGY/2020/3020	Officer:	Laurence Ackrill
Decision:	PN NOT REQ	Decision Date:	04/01/2021
Location:	3 Myddelton Road N8 7PY		
Proposal:	Application for prior approval of a proposed: Enlargement of a dwellinghouse by construction of additional storeys Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) - Schedule 2, Part 1, Class AA		
Application No:	HGY/2020/3171	Officer:	Laina Levassor
Decision:	PN NOT REQ	Decision Date:	19/01/2021
Location:	37 Rectory Gardens N8 7PJ		
Proposal:	Erection of a single storey extension which extends beyond the rear wall of the original house by 4m, for which the maximum height would be 3.2m and for which the height of the eaves would be 3m.		

RES Applications Decided: 2

Application No:	HGY/2020/3085	Officer:	Conor Guilfoyle
Decision:	REF	Decision Date:	19/01/2021
Location:	First And Second Floors 139-141 Tottenham Lane N8 9BJ		
Proposal:	Approval of details reserved by Condition 3 (cycle storage) attached to planning permission reference HGY/2020/1992.		
Application No:	HGY/2020/3088	Officer:	Conor Guilfoyle
Decision:	GTD	Decision Date:	18/01/2021
Location:	Garages opposite The Nightingale Brook Road N8		
Proposal:	Approval of details pursuant to conditions 10, parts (a) and (b) only, attached to planning permission reference HGY/2020/0159.		

Total Applications Decided for Ward: 8

WARD: Muswell Hill

CLUP Applications Decided: 1

Application No: **HGY/2020/2934** Officer: Conor Guilfoyle
 Decision: PERM DEV Decision Date: 04/01/2021
 Location: 85 Wood Vale N10 3DL
 Proposal: Certificate of lawfulness for formation of swimming pool in rear garden.

FUL Applications Decided: 9

Application No: **HGY/2020/2851** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 19/01/2021
 Location: 29 Cascade Avenue N10 3PT
 Proposal: Erection of a single storey side and rear extension (AMENDED PLANS).

Application No: **HGY/2020/2865** Officer: Conor Guilfoyle
 Decision: GTD Decision Date: 07/01/2021
 Location: 89 Muswell Hill Road N10 3HT
 Proposal: Erection of single storey side and rear extension.

Application No: **HGY/2020/2921** Officer: Fatema Begum
 Decision: GTD Decision Date: 13/01/2021
 Location: 76 Muswell Hill Place N10 3RR
 Proposal: Single storey rear extension with green flat roof, side windows and bi-folding doors.

Application No: **HGY/2020/2926** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 04/01/2021
 Location: 28 Warner Road N8 7HD
 Proposal: Construction of a single storey rear extension following the demolition of an existing single storey rear projection.

Application No: **HGY/2020/2931** Officer: Roland Sheldon
 Decision: REF Decision Date: 08/01/2021
 Location: 5 Alexandra Gardens N10 3RN
 Proposal: Erection of rear dormer with linked roof extension with side facing 2nd floor window, addition of a roof terrace, installation of platform with stairs to access the back garden (retrospective).

Application No: **HGY/2020/2965** Officer: Roland Sheldon
 Decision: GTD Decision Date: 04/01/2021
 Location: 24 Grand Avenue N10 3BB
 Proposal: Erection of single storey ground floor extension.

Application No: **HGY/2020/2974** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 04/01/2021
 Location: 177 Cranley Gardens N10 3AG
 Proposal: Construction of a single storey rear extension and extension to height of existing rear projection.

Application No: **HGY/2020/2999** Officer: Roland Sheldon
 Decision: GTD Decision Date: 05/01/2021
 Location: 31 Grand Avenue N10 3BD
 Proposal: Demolition of and erection of replacement single storey rear extension.

Application No: **HGY/2020/3153** Officer: Kwaku Bossman-Gyamera
 Decision: GTD Decision Date: 22/01/2021
 Location: 13 Barrington Road N8 8QT
 Proposal: Proposed loft conversion with rear dormers on the main roof slope and rear outrigger projection.

LBC Applications Decided: 1

Application No: **HGY/2020/2960** Officer: Tania Skelli
 Decision: GTD Decision Date: 05/01/2021
 Location: Muswell Hill Public Library Queens Avenue N10 3PE
 Proposal: Listed building consent for Internal and external alterations to existing library to include erection of glazed extension to accommodate lift shaft and installation of WC's on the ground and first floor, following partial demolition of existing single storey side addition - all to improve accessibility to all. In association with planning application ref. HGY/2020/2959.

LCD Applications Decided: 1

Application No: **HGY/2020/2959** Officer: Tania Skelli
 Decision: GTD Decision Date: 05/01/2021
 Location: Muswell Hill Public Library Queens Avenue N10 3PE
 Proposal: Internal and external alterations to existing library to include erection of glazed extension to accommodate lift shaft and installation of WC's on the ground and first floor, following partial demolition of existing single storey side addition - all to improve accessibility to all. In association with listed building consent application ref. HGY/2020/2960.

NON Applications Decided: 1

Application No: **HGY/2021/0134** Officer: Samuel Uff
 Decision: REF Decision Date: 15/01/2021
 Location: 17 Clovelly Road N8 7RR
 Proposal: Non-material amendment to HGY/2019/0612 for single storey rear infill to rear extension by virtue of extended size of extension and alteration to the door arrangement.

RES Applications Decided: 1

Application No: **HGY/2020/2981** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 13/01/2021
 Location: Risborough Close Muswell Hill N10 3PL
 Proposal: Approval of details pursuant to conditions 3 (Materials), 4 (Construction Management Plan), 5 (Green Roof), 7 (Cycle Parking) & 8 (boundary demarcation) attached to planning permission HGY/2020/1528.

Total Applications Decided for Ward: 14**WARD: Noel Park****CLUP Applications Decided: 1**

Application No: **HGY/2020/3219** Officer: Laurence Ackrill
 Decision: PERM DEV Decision Date: 04/01/2021
 Location: Russell Park 144 Russell Avenue N22 6PT
 Proposal: Certificate of lawfulness for the installation of 2 x temporary buildings within Russell Park.

FUL Applications Decided: 5

Application No: **HGY/2020/2912** Officer: Mercy Oruwari
 Decision: REF Decision Date: 06/01/2021
 Location: 188 Morley Avenue N22 6NT
 Proposal: Erection of two storey rear extension.

Application No: **HGY/2020/2996** Officer: Emily Whittredge
 Decision: GTD Decision Date: 22/01/2021
 Location: 17 High Road N22 6BH
 Proposal: Change of use from betting shop (Sui Generis) to adult gaming centre (Sui Generis)

Application No: **HGY/2020/3138** Officer: Gareth Prosser
 Decision: GTD Decision Date: 06/01/2021
 Location: Ground Floor Flat A 54 Park Ridings N8 0LD
 Proposal: Erection of single storey extension

Application No: **HGY/2020/3199** Officer: Emily Whittredge
 Decision: REF Decision Date: 19/01/2021
 Location: 109 High Road N22 6BB
 Proposal: Change of use from retail (Use Class E) to adult gaming centre (Sui Generis)

Application No: **HGY/2020/3205** Officer: Emily Whittredge
 Decision: GTD Decision Date: 22/01/2021
 Location: 57 Russell Avenue N22 6QB
 Proposal: Demolition of rear projection and erection of new rear extension and new side window with replacement front windows.

NON Applications Decided: 1

Application No: **HGY/2020/3135** Officer: Christopher Smith
 Decision: GTD Decision Date: 04/01/2021
 Location: 44-46 High Road N22 6BX
 Proposal: Non-material amendments to the wording of Conditions 3 (materials) and 10 (London Underground asset protection) of planning permission ref. HGY/2018/1472 allowed at appeal ref. APP/Y5420/W/18/3218865.

RES Applications Decided: 3

Application No: **HGY/2020/1574** Officer: Valerie Okeiyi
 Decision: GTD Decision Date: 14/01/2021
 Location: Land at Haringey Heartlands, between Hornsey Park Road, Mayes Road, Coburg Road, Western Road and the Kings Cross / East Coast Mainline, Clarendon Gas Works, Olympia Trading Estate, and 57-89 Western Road N8
 Proposal: Approval of details pursuant to condition 31 (CON2) (Land contamination) of planning permission HGY/2017/3117 relating to Building C1 only.

Application No:	HGY/2020/3132	Officer:	Christopher Smith
Decision:	GTD	Decision Date:	06/01/2021
Location:	44-46 High Road N22 6BX		
Proposal:	Approval of details pursuant to condition 5 (Air Quality and Dust Management Plan) attached to planning appeal reference APP/Y/5420/W/18/3218865 (original planning reference HGY/2018/1472).		
Application No:	HGY/2020/3133	Officer:	Christopher Smith
Decision:	GTD	Decision Date:	06/01/2021
Location:	44-46 High Road N22 6BX		
Proposal:	Approval of details pursuant to condition 6 (Non-Road Mobile Machinery) attached to planning appeal reference APP/Y/5420/W/18/3218865 (original planning reference HGY/2018/1472)		

Total Applications Decided for Ward: 10

WARD: Northumberland Park

CLDE Applications Decided: 1

Application No:	HGY/2021/0025	Officer:	Laina Levassor
Decision:	REF	Decision Date:	13/01/2021
Location:	35 Farningham Road N17 0PP		
Proposal:	Certificate of lawfulness for the existing use of the property which is currently six self contained studio flats.		

COND Applications Decided: 1

Application No:	HGY/2020/2968	Officer:	Gareth Prosser
Decision:	GTD	Decision Date:	05/01/2021
Location:	Public House 102 Northumberland Park N17 0TS		
Proposal:	Variation of condition 2 (approved plans) attached to planning consent HGY/2017/2821 to increase the development to be 0.4m higher than the original permission with minor amendments to the ground floor level related to level changes including ramps and steps.		

FUL Applications Decided: 3

Application No:	HGY/2020/2961	Officer:	Kwaku Bossman-Gyamera
Decision:	GTD	Decision Date:	04/01/2021
Location:	36 St Pauls Road N17 0NE		
Proposal:	Demolition of the existing commercial garages, removal of the existing crossover and the construction of a detached, one bedroom single storey dwelling.		
Application No:	HGY/2020/3016	Officer:	Kwaku Bossman-Gyamera
Decision:	GTD	Decision Date:	14/01/2021
Location:	835 High Road N17 8ER		
Proposal:	Erection of a 2-storey rear extension, installation of a dormer window and conversion of the existing maisonette into 2 no. self-contained flats.		
Application No:	HGY/2020/3102	Officer:	Kwaku Bossman-Gyamera
Decision:	REF	Decision Date:	08/01/2021
Location:	816-818 High Road N17 0EY		
Proposal:	New building on land adjacent to 3 Northumberland Park to create a commercial premises on the ground floor floor with 2 x one bedroom flats above.		

NON Applications Decided: 1

Application No: **HGY/2020/3112** Officer: Roland Sheldon

Decision: GTD Decision Date: 04/01/2021

Location: Land adjacent to 1-6 Romney Close N17 0NT

Proposal: Re-wording of condition 9 of planning permission HGY/2020/0183 to the following:

'Before development commences other than for investigative work:

- a. With the submitted Phase 1 Environmental Report reference 1072 - P1E - 1 - A dated 2nd March 2017 prepared by GO Contaminated Land Solutions Ltd indicating the risk of contamination and the need for Phase II investigation as alluded in sections 9 (Potential Contamination), 10 (Risk Assessment) and 13 (Conclusions) of the report, a site investigation shall be conducted for the site using information obtained from the desktop study and Conceptual Model. The investigation must be comprehensive enough to enable: a risk assessment to be undertaken, refinement of the Conceptual Model, and the development of a Method Statement detailing the remediation requirements.
- b. The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority.
- c. If the risk assessment and refined Conceptual Model indicate any risk of harm, a Method Statement detailing the remediation requirements, using the information obtained from the site investigation, and also detailing any post remedial monitoring shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site.
- d. Where remediation of contamination on the site is required, completion of the remediation detailed in the method statement shall be carried out and a report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority before the development is occupied.

Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety in accordance with Policy 5.21 of the London Plan 2011 and Policy DM1 of The Development Management DPD 2017.'

PNC Applications Decided: 1

Application No: **HGY/2020/2862** Officer: Kevin Tohill

Decision: APPROVED Decision Date: 06/01/2021

Location: Former Haringey Irish Centre Pretoria Road N17 8DX

Proposal: Use of Class Q Permitted Development Rights under The Town and Country Planning (General Permitted Development) Order 2015 for part of the Car Park at the Former Haringey Irish Centre, Pretoria Road, Tottenham, London N17 8DX (the "Site")

PNE Applications Decided: 1

Application No: **HGY/2020/3172** Officer: Laina Levassor

Decision: PN NOT REQ Decision Date: 19/01/2021

Location: 6 Almond Road N17 0PJ

Proposal: Erection of a single storey extension which extends beyond the rear wall of the original house by 3.6m, for which the maximum height would be 3.3m and for which the height of the eaves would be 3m.

RES Applications Decided: 6

Application No: **HGY/2020/2716** Officer: Roland Sheldon

Decision: GTD Decision Date: 13/01/2021

Location: Land adjacent to 1-6 Romney Close N17 0NT

Proposal: Discharge of condition 12 (Arboricultural Statement and Tree Protection Plan) of planning permission HGY/2020/0183.

Application No: **HGY/2020/2886** Officer: Roland Sheldon
 Decision: GTD Decision Date: 12/01/2021
 Location: Land adjacent to 1-6 Romney Close N17 0NT
 Proposal: Discharge of conditions 9a and 9b (contamination) planning permission HGY/2020/0183.

Application No: **HGY/2020/3002** Officer: Roland Sheldon
 Decision: GTD Decision Date: 13/01/2021
 Location: Land adjacent to 1-6 Romney Close N17 0NT
 Proposal: Discharge of condition 9c (remediation strategy) of planning permission HGY/2020/0183.

Application No: **HGY/2020/3065** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 21/01/2021
 Location: 73 Manor Road N17 0JH
 Proposal: Approval of details pursuant to condition 8 (Written scheme for the treatment of the surroundings) attached to planning consent HGY/2017/3306

Application No: **HGY/2020/3115** Officer: Neil McClellan
 Decision: GTD Decision Date: 07/01/2021
 Location: Land rear of 705-707 High Road N17 8AD
 Proposal: Approval of details pursuant to Condition 3 (Protection of existing boundary walls) of Listed Building Consent Ref: HGY/2018/2279.

Application No: **HGY/2020/3117** Officer: Neil McClellan
 Decision: GTD Decision Date: 07/01/2021
 Location: Land rear of 705-707 High Road N17 8AD
 Proposal: Approval of details pursuant to Condition 5 (Archaeological survey), Condition 8 (Air quality and dust management plan), Condition 18 (Protection of existing boundary walls) of Planning Permission Ref: HGY/2020/0533.

Total Applications Decided for Ward: 14

WARD: **St Anns**

CLUP Applications Decided: 1

Application No: **HGY/2020/2993** Officer: Emily Whittredge
 Decision: PERM DEV Decision Date: 12/01/2021
 Location: 10 Gorleston Road N15 5QR
 Proposal: Certificate of lawfulness for the erection of a single storey rear extension.

FUL Applications Decided: 3

Application No: **HGY/2020/2471** Officer: Gareth Prosser
 Decision: GTD Decision Date: 08/01/2021
 Location: 2A Kimberley Gardens N4 1LF
 Proposal: Erection of second floor extension with terraces to create 2x 1bed self contained flats and associated landscaping to mews.

Application No: **HGY/2020/2480** Officer: Gareth Prosser
 Decision: GTD Decision Date: 13/01/2021
 Location: 25 Avenue Road N15 5JG
 Proposal: Erection of first-floor rear extension.

Application No: **HGY/2020/3010** Officer: Sarah Madondo
 Decision: GTD Decision Date: 11/01/2021
 Location: 22 Avenue Road N15 5JH
 Proposal: Change of use to 4 no self contained flats. Ground and first floor rear extensions. Replace existing windows. Front and rear dormer windows.

PNC Applications Decided: 1

Application No: **HGY/2020/2911** Officer: Mercy Oruwari
 Decision: PN REFUSED Decision Date: 05/01/2021
 Location: 449 West Green Road N15 3PL
 Proposal: An application to determine if prior approval is required for a proposed Change of use from retail (Class A1) to residential use (Class C3).

RES Applications Decided: 2

Application No: **HGY/2020/2628** Officer: Roland Sheldon
 Decision: REF Decision Date: 19/01/2021
 Location: 10 Gourley Street N15 5NG
 Proposal: Discharge of condition 4 (construction management plan) of planning application HGY/2017/2458.

Application No: **HGY/2021/0121** Officer: Roland Sheldon
 Decision: GTD Decision Date: 19/01/2021
 Location: 10 Gourley Street N15 5NG
 Proposal: Discharge of condition 5 (tree protection plan) of planning application HGY/2017/2458.

Total Applications Decided for Ward: 7WARD: **Seven Sisters****CLDE Applications Decided: 1**

Application No: **HGY/2020/3099** Officer: Mercy Oruwari
 Decision: REF Decision Date: 19/01/2021
 Location: 4 Lockmead Road N15 6BX
 Proposal: Certificate of Lawfulness for existing works including rear ground and first floor extensions, rear dormer, front lightwell and basement works.

CLUP Applications Decided: 2

Application No: **HGY/2020/2908** Officer: Laina Levassor
 Decision: PERM REQ Decision Date: 04/01/2021
 Location: 190 Hermitage Road N4 1NN
 Proposal: Certificate of Lawfulness for proposed outrigger extension.

Application No: **HGY/2021/0023** Officer: Tobias Finlayson
 Decision: PERM DEV Decision Date: 15/01/2021
 Location: 12 Oakdale Road N4 1NX
 Proposal: Certificate of lawfulness (proposed use) for erection of a rear dormer and insertion of 4 front rooflights to facilitate a loft conversion under 40 cubic metres in matching materials.

FUL Applications Decided: 9

Application No: **HGY/2020/2916** Officer: Laina Levassor
 Decision: REF Decision Date: 04/01/2021
 Location: 190 Hermitage Road N4 1NN
 Proposal: Construction of rear dormer to facilitate loft conversion.

Application No: **HGY/2020/2918** Officer: Laina Levassor
 Decision: REF Decision Date: 14/01/2021
 Location: 190 Hermitage Road N4 1NN
 Proposal: Erection of first floor rear extension.

Application No: **HGY/2020/2950** Officer: Laina Levassor
 Decision: REF Decision Date: 14/01/2021
 Location: 190 Hermitage Road N4 1NN
 Proposal: Erection of single storey rear extension.

Application No: **HGY/2020/3014** Officer: Kwaku Bossman-Gyamera
 Decision: GTD Decision Date: 12/01/2021
 Location: 56 Rostrevor Avenue N15 6LP
 Proposal: Erection of a first floor rear extension.

Application No: **HGY/2020/3047** Officer: Gareth Prosser
 Decision: GTD Decision Date: 06/01/2021
 Location: 94 Fairview Road N15 6TP
 Proposal: Change of use from two flats to single dwelling house.

Application No: **HGY/2020/3060** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 14/01/2021
 Location: Flat B 31 Daleview Road N15 6PL
 Proposal: Erection of a rear dormer for top floor flat including the insertion of 3 front rooflights.

Application No: **HGY/2020/3094** Officer: Sarah Madondo
 Decision: GTD Decision Date: 20/01/2021
 Location: 43-47 Oakdale Road N4 1NU
 Proposal: Part single part two-storey extension to the rear of properties at 43, 45 and 47 Oakdale Road.

Application No: **HGY/2020/3126** Officer: Sarah Madondo
 Decision: GTD Decision Date: 15/01/2021
 Location: 31 Norfolk Avenue N15 6JX
 Proposal: Erection of Type 3 roof extension.

Application No: **HGY/2020/3143** Officer: Gareth Prosser
 Decision: GTD Decision Date: 20/01/2021
 Location: 34 Vartry Road N15 6PU
 Proposal: Loft conversion with raised rear roof to main building and part raised roof to rear extension.

NON Applications Decided: 1

Application No: **HGY/2021/0083** Officer: Kwaku Bossman-Gyamera
 Decision: GTD Decision Date: 18/01/2021
 Location: 58-60 High Road N15 6JU
 Proposal: Non-material amendment following a grant of planning permission HGY/2019/3330 involving the following changes: secondary means of egress in case of emergency (fire) due to its inherent deep plan.

PNC Applications Decided: 1

Application No: **HGY/2020/2930** Officer: Samuel Uff
 Decision: PN REFUSED Decision Date: 08/01/2021
 Location: Roeder House Vale Road N4 1QE
 Proposal: Prior Approval for a Proposed Change of Use of a building from Office Use (Class B1(a)) to create 29 residential units (Class C3)

PNE Applications Decided: 4

Application No: **HGY/2020/3000** Officer: Laina Levassor
 Decision: PN NOT REQ Decision Date: 05/01/2021
 Location: 135 Castlewood Road N15 6BD
 Proposal: Erection of single storey extension which extends beyond the rear wall of the original house by 6m, for which the maximum height would be 3.5m and for which the height of the eaves would be 3m.

Application No: **HGY/2020/3116** Officer: Kwaku Bossman-Gyamera
 Decision: PN REFUSED Decision Date: 07/01/2021
 Location: 2 Lemsford Close N15 6BY
 Proposal: Application to determine if prior approval is required for a proposed: Enlargement of a dwellinghouse by construction of additional storeys under Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) - Schedule 2, Part 1, Class AA

Application No: **HGY/2020/3177** Officer: Laina Levassor
 Decision: PN REFUSED Decision Date: 13/01/2021
 Location: 12 Oakdale Road N4 1NX
 Proposal: Erection of single storey extension which extends beyond the rear wall of the original house by 6m, for which the maximum height would be 3m and for which the height of the eaves would be 3.3m

Application No: **HGY/2020/3208** Officer: Laina Levassor
 Decision: PN REFUSED Decision Date: 22/01/2021
 Location: 35 Oakdale Road N4 1NU
 Proposal: Erection of single storey extension which extends beyond the rear wall of the original house by 6m, for which the maximum height would be 3m and for which the height of the eaves would be 3m

Total Applications Decided for Ward: 18

WARD: Stroud Green

COND Applications Decided: 1

Application No: **HGY/2020/3151** Officer: Conor Guilfoyle

Decision: GTD Decision Date: 19/01/2021

Location: 79 Ridge Road N8 9NP

Proposal: Variation of condition 2 (approved plans) attached to planning permission HGY/2020/1029 to increase the size of the basement level, ground floor, and first floor of the new house in the rear garden, with associated external alterations.

FUL Applications Decided: 7

Application No: **HGY/2020/2954** Officer: Sarah Madondo

Decision: REF Decision Date: 14/01/2021

Location: 26 Ferme Park Road N4 4ED

Proposal: Part excavation of existing basement to provide a new self-contained flat with formation of terrace and boiler room (amended description)

Application No: **HGY/2020/2972** Officer: Laurence Ackrill

Decision: GTD Decision Date: 11/01/2021

Location: 40 Ferme Park Road N4 4ED

Proposal: Construction of rear lightwell, replacement rear dormer roof extension and rear inset first floor balcony to facilitate the conversion of single family dwellinghouse into 4 x self-contained flats.

Application No: **HGY/2020/2991** Officer: Fatema Begum

Decision: GTD Decision Date: 22/01/2021

Location: 102 Stapleton Hall Road N4 4QA

Proposal: Garden Studio Outbuilding (AMENDED PLANS)

Application No: **HGY/2020/3042** Officer: Matthew Gunning

Decision: GTD Decision Date: 07/01/2021

Location: 42 Nelson Road N8 9RU

Proposal: Proposed single storey rear extension

Application No: **HGY/2020/3054** Officer: Mercy Oruwari

Decision: GTD Decision Date: 08/01/2021

Location: 111 Mount View Road N4 4JH

Proposal: Installation of new lower bay windows and lower ground side elevation window and associated landscaping works.

Application No: **HGY/2020/3097** Officer: Roland Sheldon

Decision: REF Decision Date: 19/01/2021

Location: 33 Upper Tollington Park N4 3EJ

Proposal: Single storey ground floor side to rear extension in association with conversion of the property into 3 self-contained flats.

Application No: **HGY/2020/3118** Officer: Laurence Ackrill

Decision: GTD Decision Date: 11/01/2021

Location: 34 Mount Pleasant Crescent N4 4HP

Proposal: Construction of a single storey rear extension following the demolition of existing rear projections.

NON Applications Decided: 1

Application No: **HGY/2020/3262** Officer: Samuel Uff
 Decision: GTD Decision Date: 04/01/2021
 Location: Flat A 76 Upper Tollington Park N4 4NB
 Proposal: Non-material amendment to permission HGY/2019/3252 change from render to brick; part increased height; installation of arch above and re-siting of patio doors; and amended raised rooflights.

Total Applications Decided for Ward: 9WARD: **Tottenham Green****FUL Applications Decided: 6**

Application No: **HGY/2020/2896** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 04/01/2021
 Location: Flat 18 Esin Court Broad Lane N15 4QR
 Proposal: Alteration and installation of front and rear roof velux windows

Application No: **HGY/2020/3007** Officer: Kwaku Bossman-Gyamera
 Decision: REF Decision Date: 06/01/2021
 Location: 2-8 Walton Road N15 4PP
 Proposal: Installation of slimline aluminium framed retractable glazed front and side extensions.

Application No: **HGY/2020/3052** Officer: Gareth Prosser
 Decision: REF Decision Date: 06/01/2021
 Location: 2 Wakefield Road N15 4NL
 Proposal: Demolition of existing garages and erection of a part-1 and part-2 storey building to provide 2 residential dwellings and associated landscaping and development.

Application No: **HGY/2020/3095** Officer: Sarah Madondo
 Decision: GTD Decision Date: 13/01/2021
 Location: Blarney Villa 17 Clyde Circus N15 4LF
 Proposal: Demolition of existing single storey rear outhouse and erection of single storey rear extension to kitchen/dining room and side extension to accommodate a ground floor shower room.

Application No: **HGY/2020/3100** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 19/01/2021
 Location: 28B West Green Road N15 5NP
 Proposal: Erection of a rear dormer for top floor flat including the insertion of 2 front rooflights

Application No: **HGY/2020/3119** Officer: Roland Sheldon
 Decision: REF Decision Date: 20/01/2021
 Location: Flat A 80 Elmar Road N15 5DJ
 Proposal: Creation of mansard roof extension to existing two storey building to provide additional living space for upper floor flat.

NON Applications Decided: 1

Application No: **HGY/2020/3233** Officer: Valerie Okeiyi
 Decision: GTD Decision Date: 05/01/2021
 Location: 45-63 Lawrence Road N15 4EN
 Proposal: Non-material amendment following a grant of planning permission HGY/2016/1213 to amend the trigger point of conditions 4 (Boundary Treatment) , 6 (Hard and soft landscaping works), 7 (New trees and shrubs) , 5 (Details of all levels on site).

RES Applications Decided: 2

Application No: **HGY/2020/2836** Officer: Valerie Okeiyi
 Decision: GTD Decision Date: 05/01/2021
 Location: 45-63 Lawrence Road N15 4EN
 Proposal: Approval of details pursuant to condition 5a (ground levels) attached to planning permission HGY/2016/1213.

Application No: **HGY/2020/3136** Officer: Christopher Smith
 Decision: GTD Decision Date: 19/01/2021
 Location: Apex House 820 Seven Sisters Road N15 5PQ
 Proposal: Approval of details pursuant to condition 12 (remediation of contamination) attached to planning permission HGY/2015/2915.

TEL Applications Decided: 1

Application No: **HGY/2020/3103** Officer: Kwaku Bossman-Gyamera
 Decision: PN GRANT Decision Date: 15/01/2021
 Location: Cordell House Newton Road N15 4PR
 Proposal: Upgrade of existing telecommunications base station. Removal and replacement of 6no. antennas, internal upgrade of existing equipment room and associated ancillary works thereto. (Prior notification: Development by telecoms operators)

Total Applications Decided for Ward: 10WARD: **Tottenham Hale****COND Applications Decided: 1**

Application No: **HGY/2020/1920** Officer: Christopher Smith
 Decision: GTD Decision Date: 14/01/2021
 Location: Bus Garage and Depot Marsh Lane N17 0UX
 Proposal: Removal of condition 3 attached to planning permission HGY/2017/2306.

FUL Applications Decided: 2

Application No: **HGY/2020/3029** Officer: Sarah Madondo
 Decision: GTD Decision Date: 15/01/2021
 Location: 166 Shelbourne Road N17 9YA
 Proposal: Single storey and part two storey rear extension with new window openings to front and rear.

Application No: **HGY/2020/3093** Officer: Sarah Madondo
 Decision: GTD Decision Date: 14/01/2021
 Location: 166-168 Shelbourne Road N17 9YA
 Proposal: Creation of additional second floor to dwelling together with new window openings to front and rear.

NON Applications Decided: 2

Application No:	HGY/2020/1973	Officer:	Philip Elliott
Decision:	GTD	Decision Date:	22/01/2021
Location:	2 Chesnut Road N17 9EN		
Proposal:	Amendment of proposal description of planning permission HGY/2013/0155 to "Partial demolition of existing buildings, retaining existing historical facade, construction of accommodation over 3 and 4 floors to provide 64 residential rooms and amenities areas"		
Application No:	HGY/2020/2806	Officer:	Philip Elliott
Decision:	GTD	Decision Date:	08/01/2021
Location:	Ashley Gardens Ashley Road N17 9LJ		
Proposal:	Non-Material Amendment to Planning Permission HGY/2019/2804 (which amended HGY/2017/2045) to allow for internal changes (with corresponding external alterations and additions & changes to Conditions 4 (Land Use (Retail) and 45 (Quantum of Development)) to increase amenity provision and modify the design. The proposed amendments reduce the quantum of residential units by 1.1% from 423 to 418 units. The number of affordable housing units remains unchanged.		

PNC Applications Decided: 1

Application No:	HGY/2020/3152	Officer:	Gareth Prosser
Decision:	PN REFUSED	Decision Date:	14/01/2021
Location:	28-39 Bronhill Terrace N17 0LN		
Proposal:	Application to determine if prior approval is required for a proposed: New dwellinghouses on detached buildings in commercial or mixed use - Two storey roof extension above principal building to create 8 additional dwellings. Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) - Schedule 2, Part 20, Class AA		

RES Applications Decided: 8

Application No:	HGY/2020/2438	Officer:	Martin Cowie
Decision:	GTD	Decision Date:	08/01/2021
Location:	Strategic Development Partnership (SDP) Sites Welbourne, North Island, Ferry Island, Ashley Road East and Ashley Road West, Station Road N17		
Proposal:	Application for the approval of details pursuant to condition D32 (Ashley Road Facade) in relation to Plot D (Ashley Road West site) of the Tottenham Hale Centre planning permission ref. HGY/2018/2223 dated 27th March 2019.		
Application No:	HGY/2020/2945	Officer:	Tobias Finlayson
Decision:	GTD	Decision Date:	13/01/2021
Location:	Land north of Monument Way and South of Fairbanks Road N17		
Proposal:	Approval of details pursuant to condition 14 (Ventilation Strategy) attached to planning permission HGY/2018/0050.		
Application No:	HGY/2020/3025	Officer:	Philip Elliott
Decision:	GTD	Decision Date:	08/01/2021
Location:	Ashley Gardens Ashley Road N17 9LJ		
Proposal:	Approval of details pursuant to condition 42b (satellite dish system) attached to planning permission HGY/2019/2804.		
Application No:	HGY/2020/3087	Officer:	Conor Guilfoyle
Decision:	GTD	Decision Date:	18/01/2021
Location:	Garage Colony St Marys Close N17		
Proposal:	Approval of details pursuant to parts (a) and (b) of condition 9 (remediation strategy) of planning permission reference HGY/2020/0136		

Application No:	HGY/2020/3175	Officer:	Martin Cowie
Decision:	GTD	Decision Date:	22/01/2021
Location:	Strategic Development Partnership (SDP) Sites Welbourne, North Island, Ferry Island, Ashley Road East and Ashley Road West Station Road N17		
Proposal:	Application for the full approval of details pursuant to Conditions A19 (Monitoring and Maintenance Plan - Contamination - Environment Agency) and A24 (Contaminated Land - Part 1 - LBH Environmental Health) in relation to Plot A (North Island site) of the Tottenham Hale Centre development Planning Permission (LPA ref: HGY/2018/2223) dated 27th March 2019 (Conditions A19 and A24 have already been part discharged to allow demolition works to proceed - LPA refs: HGY/2020/0274 and HGY/2020/0275 respectively).		
Application No:	HGY/2020/3176	Officer:	Christopher Smith
Decision:	GTD	Decision Date:	08/01/2021
Location:	SW Plot Hale Village Ferry Lane N17		
Proposal:	Approval of details pursuant to condition 33 (Electric Vehicle Charging Points) attached to planning permission HGY/2017/2005.		
Application No:	HGY/2020/3215	Officer:	Martin Cowie
Decision:	GTD	Decision Date:	08/01/2021
Location:	Strategic Development Partnership (SDP) Sites Welbourne, North Island, Ferry Island, Ashley Road East and Ashley Road West Station Road N17		
Proposal:	Application for the partial approval of details (bricks/mortar only) pursuant to Condition D1 (Materials samples - LBH Development Management) in relation to Plot D (Ashley Road West site) of the Tottenham Hale Centre development Planning Permission (LPA ref. HGY/2018/2223) dated 27th March 2019.		
Application No:	HGY/2020/3246	Officer:	Christopher Smith
Decision:	GTD	Decision Date:	13/01/2021
Location:	Marsh Lane Refuse Depot Marsh Lane N17 0XB		
Proposal:	Approval of details pursuant to conditions 6 (Construction Management Plan and Construction Logistics Plan - discharge of final part) and 25 (alterations / improvements to affected roads, routes and crossings) attached to planning permission HGY/2019/1278.		

Total Applications Decided for Ward: 14

WARD: West Green

COND Applications Decided: 1

Application No:	HGY/2020/3130	Officer:	Gareth Prosser
Decision:	REF	Decision Date:	06/01/2021
Location:	300 Philip Lane N15 4AB		
Proposal:	Removal / variation of conditions 2 (approved plans) and 4 (cycle parking) attached to permission HGY/2019/1467		

FUL Applications Decided: 7

Application No:	HGY/2020/2800	Officer:	Mercy Oruwari
Decision:	GTD	Decision Date:	19/01/2021
Location:	83 Carlingford Road N15 3EJ		
Proposal:	Erection of a single storey side and rear extension.		
Application No:	HGY/2020/3059	Officer:	Mercy Oruwari
Decision:	GTD	Decision Date:	14/01/2021
Location:	280A Philip Lane N15 4AB		
Proposal:	Installation of 2x velux windows and replacement of 1x existing velux into the pitched roof at the rear. Installation of 1 x rooflight on the rear flat roof.		

Application No: **HGY/2020/3068** Officer: Emily Whittredge
 Decision: GTD Decision Date: 12/01/2021
 Location: 211 Downhills Way N17 6AH
 Proposal: Outbuilding ancillary to main dwelling house with flat roof.

Application No: **HGY/2020/3086** Officer: Emily Whittredge
 Decision: GTD Decision Date: 04/01/2021
 Location: First Floor Flat B 116 Langham Road N15 3LX
 Proposal: L shaped dormer loft conversion with roof lights to front slope. Replacement of existing uPVC windows on first floor.

Application No: **HGY/2020/3101** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 20/01/2021
 Location: 3 Walpole Road N17 6BE
 Proposal: Erection of single storey outbuilding in the rear garden.

Application No: **HGY/2020/3127** Officer: Gareth Prosser
 Decision: REF Decision Date: 05/01/2021
 Location: 136 Sirdar Road N22 6RD
 Proposal: Erection of roof terrace on flat roof accessible via bi-fold doors, from the bedroom of the loft conversion previously approved, with minor amendments to approved planning application.

Application No: **HGY/2020/3167** Officer: Gareth Prosser
 Decision: REF Decision Date: 21/01/2021
 Location: Land to rear of 195 Sirdar Road N22 6QU
 Proposal: Demolition of existing garages and erection of new dwellinghouse.

LCD Applications Decided: 1

Application No: **HGY/2020/1599** Officer: Christopher Smith
 Decision: GTD Decision Date: 08/01/2021
 Location: Rochford Griffin Road N17 6HX
 Proposal: Erection of enclosed brick built electrical substation below Rochford block.

TEL Applications Decided: 1

Application No: **HGY/2020/3283** Officer: Kwaku Bossman-Gyamera
 Decision: PERM DEV Decision Date: 08/01/2021
 Location: Downhills Park Road DNS Downhills Park Road N17
 Proposal: Formal notification in writing of 28 days notice in advance, in accordance with Regulation 5 of the Electronic Communications Code (Conditions and Restrictions) Regulations 2003 (as amended)
 Description of Development: The removal and replacement of 2No. BTS3900A cabinets (1400 x 600 x 480mm) with 1No. Porter cabinet (1452 x 1450 x 650mm), the installation of 1No. GPS unit affixed to the monopole and ancillary development thereto.

Total Applications Decided for Ward: 10**WARD: White Hart Lane****FUL Applications Decided: 1**

Application No: **HGY/2020/2899** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 05/01/2021
 Location: 154 Risley Avenue N17 7ER
 Proposal: Erection of a single storey rear extension and conversion of the loft including the installation of 2 rear roof lights.

RES Applications Decided: 1

Application No: **HGY/2020/3252** Officer: Laurence Ackrill
 Decision: GTD Decision Date: 14/01/2021
 Location: 555 White Hart Lane N17 7RP
 Proposal: Approval of details pursuant to condition 10 (External lighting) attached to planning permission HGY/2020/0635.

Total Applications Decided for Ward: 2WARD: **Woodside****CLUP Applications Decided: 2**

Application No: **HGY/2020/2913** Officer: Mercy Oruwari
 Decision: PERM DEV Decision Date: 06/01/2021
 Location: 21 Berners Road N22 5NE
 Proposal: Certificate of lawfulness for erection of a rear dormer and roof extension including the insertion of 2 front rooflights and Juliet balcony.

Application No: **HGY/2020/2914** Officer: Mercy Oruwari
 Decision: PERM REQ Decision Date: 07/01/2021
 Location: 21 Berners Road N22 5NE
 Proposal: Certificate of lawfulness for conversion of existing garage into a habitable room - proposed use.

FUL Applications Decided: 5

Application No: **HGY/2020/2559** Officer: Kwaku Bossman-Gyamera
 Decision: REF Decision Date: 05/01/2021
 Location: 49 The Roundway N17 7HA
 Proposal: Demolition of an existing canopy and the erection of a timber pergola with a 1-metre high timber perimeter fence in front yard of shop.

Application No: **HGY/2020/3120** Officer: Emily Whittredge
 Decision: GTD Decision Date: 06/01/2021
 Location: St Cuthberts Church Wolves Lane N22 5JD
 Proposal: Demolition of the existing boiler room to the south of the church and replacement with a timber structure with felt roof, located between the buttresses.

Application No: **HGY/2020/3169** Officer: Gareth Prosser
 Decision: GTD Decision Date: 08/01/2021
 Location: 6 Eldon Road N22 5DT
 Proposal: Erection of single storey rear/side extension.

Application No: **HGY/2020/3200** Officer: Emily Whittredge
 Decision: GTD Decision Date: 19/01/2021
 Location: 494 Lordship Lane N22 5DE
 Proposal: Variation of existing opening hours of cafe to 07:00 - 23:30 Monday to Sunday and Bank Holidays

Application No: **HGY/2020/3247** Officer: Laina Levassor
 Decision: GTD Decision Date: 22/01/2021
 Location: 63 The Roundway N17 7HB
 Proposal: Formation of dropped kerb and crossover.

NON Applications Decided: 1

Application No: **HGY/2021/0071** Officer: Mercy Oruwari
 Decision: GTD Decision Date: 15/01/2021
 Location: Flat A 20 Arcadian Gardens N22 5AA
 Proposal: Non-material amendment application following a grant of planning permission HGY/2020/2511 to alter the rear and side elevation external glazed doors and windows. Including the relocation and change in materials to aluminium or steel with Crittall-look appearance.

TEL Applications Decided: 1

Application No: **HGY/2020/3019** Officer: Kwaku Bossman-Gyamera
 Decision: PN REFUSED Decision Date: 14/01/2021
 Location: Elizabeth Blackwell House Progress Way N22 5PB
 Proposal: Removal of 7no. existing antennas and the installation of 6no. new antennas, the installation of 3 No. cabinets and the removal of 6 No. existing cabinets (4 No. total remaining) and associated ancillary works thereto. (Prior notification: Development by telecoms operators)

Total Applications Decided for Ward: 9

WARD: **Not Applicable - Outside Borough**

OBS Applications Decided: 2

Application No: **HGY/2020/3046** Officer: Matthew Gunning
 Decision: RNO Decision Date: 18/01/2021
 Location: 100 Amhurst Park N16 5AR
 Proposal: Erection of a single-storey side extension at lower ground floor with rooftop terrace; erection of three-storey stair core at ground, first and second floor levels; installation of rooflights; and associated works to elevations including replacement, installation and infilling of windows and doors to facilitate the change of use from a place of worship (Use Class F1(f)) to a school (Use Class F1(a)). (Observations to L.B. Hackney - their reference 2020/3272)

Application No: **HGY/2021/0055** Officer: Matthew Gunning
 Decision: RNO Decision Date: 12/01/2021
 Location: 203 Middleham Road N18 2RY
 Proposal: Sub-division and conversion of premises into 2 single family dwellings involving front and rear rooflights together with associated parking, refuse and recycling. (Observations to the London Borough of Enfield - 20/03983/FUL)

Total Applications Decided for Ward: 2**Total Number of Applications Decided: 210**

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